

LIVE. WORK. PLAY.



## Fiscal Year 2021 Ten Year Capital Improvement Plan

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# Introduction

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## *County Commissioners of Washington County*

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JEFFREY A. "JEFF" CLINE, a third-term County Commissioner, serves as President of the Board of County Commissioners and is a Williamsport, Maryland, resident. Mr. Cline is a graduate of Williamsport High School and Hagerstown Community College. An experienced realtor since 2003, Mr. Cline has assumed an inactive status in this field in order to fulfill his commitment to Washington County. He graduated from the Maryland Association of Realtors' 2008 Leadership Academy, received the Graduate of Realtor Institute (GRI) designation and is also a graduate of Leadership Washington County Class 26. Mr. Cline served on the Williamsport Town Council from 2005 to 2009.



TERRY L. BAKER, a fourth-term County Commissioner, was first elected in 2006, and serves as Vice-President of the Board of County Commissioners. Mr. Baker is a 1973 graduate of Williamsport High School, a 1975 graduate of Hagerstown Community College and a 1978 graduate of Auburn University, with a bachelor's degree in Education. Mr. Baker retired in 2015 from the position of Washington County Students Trades Coordinator for the Washington County Technical High School after being an educator for 34 years. Prior to being elected a County Commissioner he served from 2002 to 2004 as a member of the Council for the municipality of Clear Spring, Maryland, and as Assistant Mayor for such municipality from 2004 to 2006.



WAYNE K. KEEFER, a second-term County Commissioner, was appointed to fill a vacancy on the Board of County Commissioners on March 25, 2016 by Maryland Governor Lawrence J. Hogan, Jr., initially assumed office on April 5, 2016 and was subsequently elected to a full, four-year term in 2018. Mr. Keefer is a lifelong resident of Hancock and a 2004 graduate of Hancock Middle-Senior High School. Mr. Keefer holds an A.S. degree in Management from Hagerstown Community College and a B.S. degree in Business Administration and an M.B.A. from Frostburg State University. He has over a decade of experience as a commercial banker and is currently a small business owner and an adjunct instructor with Frostburg State University and the University System of Maryland at Hagerstown.



CORT F. MEINELSCHMIDT, a first-term County Commissioner, was born and raised in Washington County. Mr. Meinelschmidt served in the United States Navy as a rescue swimmer for nearly seven years. On September 30, 2003, the Department of the Navy conferred upon Mr. Meinelschmidt the Navy and Marine Corps Achievement Medal for "the superior performance of his duties" and "more notably, 15 lives saved." He has worked in the financial services business since 2004 and is currently the CEO of Sentinel Capital Solutions, where he is active in the day-to-day research and trading of Sentinel's investment portfolios. Mr. Meinelschmidt is very active in the community serving on non-profit boards and as a coach to Washington County Special Olympics Swim Team.



RANDALL E. "RANDY" WAGNER, a first-term County Commissioner, was born and raised in Washington County. Mr. Wagner graduated from North Hagerstown High School and is a veteran of the United States Coast Guard, where he served for four years. Mr. Wagner worked at Mack Truck for 17 years before becoming a small business owner in Washington County, owning and operating 40 West Marine in Clear Spring for eight years. He has served the local community as a realtor for the past 16 years and is a licensed private pilot. Prior to his election to the Board of County Commissioners, Mr. Wagner served on the Animal Control Board of Washington County for eight years and held the position of Vice-Chair.

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## VISION

The vision of Washington County Government is to become the regional leader in providing and coordinating efficient and effective public services in an open and cooperative manner.

## MISSION

Supporting and strengthening individual and community self-reliance and responsibility;

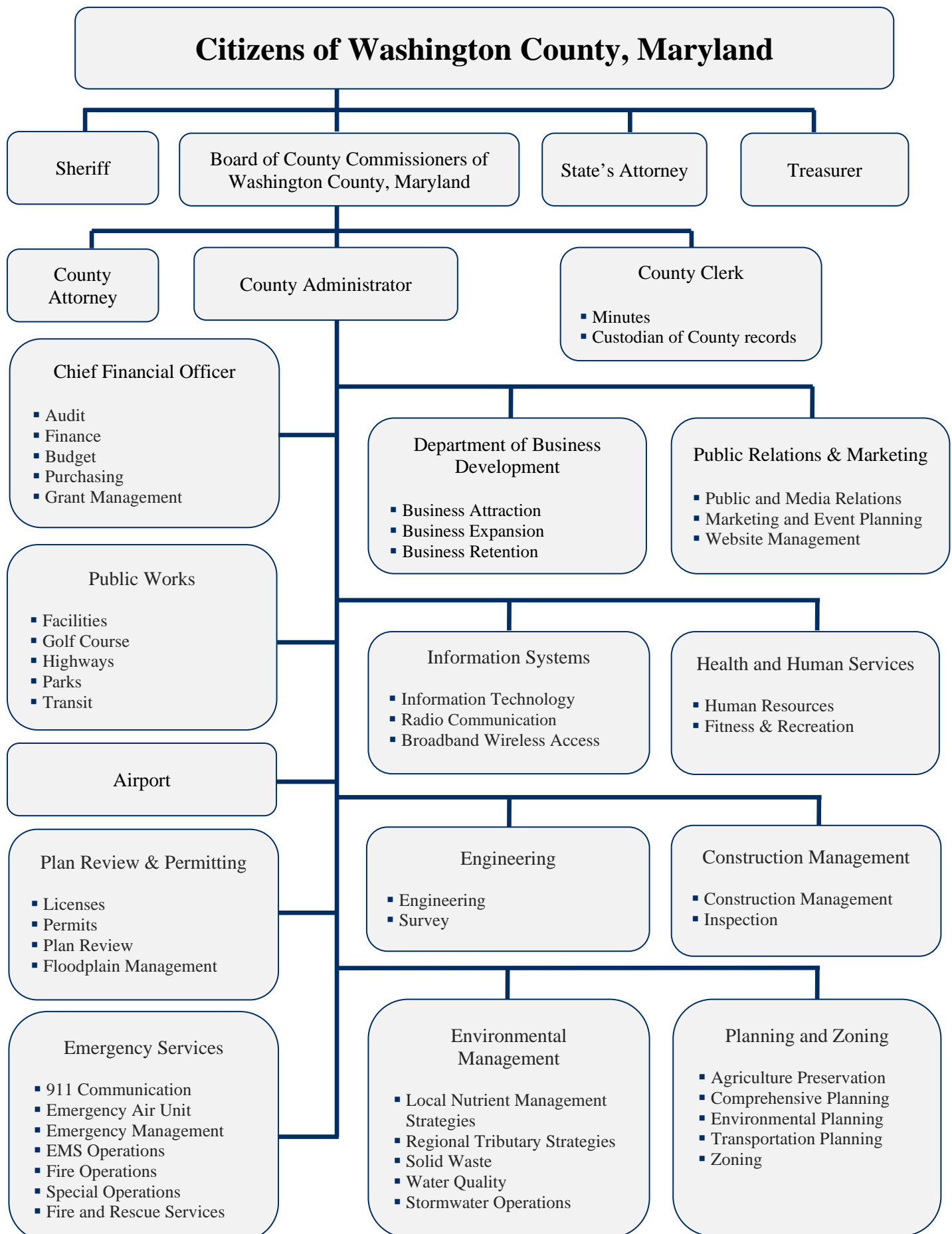
Promoting education, economic opportunities, public health, safety, and welfare;

Protecting the environment and cultural resources we share and;

Planning for future urbanization and a culturally diverse population.

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## Community Profile

### Location

Washington County is situated in northwestern Maryland, bordered by Pennsylvania to the north and West Virginia to the south. It is bordered on the east by Frederick County, Maryland and on the west by Allegany County, Maryland. Washington County is approximately 460 square miles in area. The County Seat, Hagerstown, is 70 miles northwest of Washington, D.C. Two major highways, Interstate 81 – running north and south, and Interstate 70 – running east and west, cross within the County's borders.



The major part of Washington County is fertile valley with rolling terrain. The lowland belt known as the Hagerstown Valley, which lies between the Blue Ridge Mountains to the west and the Appalachian ridges to the east, is an extension of the Shenandoah Valley of Virginia.

Washington County is a great place, not only for locating a business, but also for living and raising a family. The rural, friendly, community environment offers a taste of traditional values – and the close proximity to Baltimore and Washington D.C. offers access to a metropolitan culture and lifestyle. It's the best of both worlds.

### Culture and Recreation

Founded in 1776, Washington County was the first county in the United States to be named for the then General George Washington. The history of Washington County is exhibited at 4 national parks, 7 state parks, 14 county parks, numerous monuments and more than 35 museums that tell America's story spanning three centuries.

Some of Washington County's major local attractions include:



- ❖ Antietam National Battlefield – the site of one of the most famous Civil War battles.
- ❖ Fort Frederick State Park – a fort built in 1756 for use during the French and Indian War.
- ❖ Appalachian National Scenic Trail – the trail in Maryland follows a 40 mile route along the backbone of South Mountain (a north-side ridge that extends from Pennsylvania to the Potomac River).
- ❖ C&O Canal National Historical Park – for nearly 100 years the canal served as a waterway to transport products and it now provides a place to relax and enjoy nature.

- ❖ Maryland Symphony Orchestra – Western Maryland’s only professional orchestra.
- ❖ Hagerstown Speedway – one of the fastest and safest dirt tracks in the United States.
- ❖ Hagerstown Suns – a minor South Atlantic League Class A baseball team affiliated with the Washington Nationals.
- ❖ Washington County Museum of Fine Arts – with a permanent collection of over 6,000 works of art, as well as changing exhibitions.
- ❖ The Maryland Theatre – built in 1915; home of the Maryland Symphony Orchestra and the Miss Maryland Pageant.

Hagerstown was founded by German immigrant Jonathan Hager who was a volunteer Captain of Scouts during the French and Indian War. Located in the center of the Great Valley in Western Central Maryland, Hagerstown was at the crossroads of the Civil War. The Valley provided a natural corridor for refugee and troop movements between Virginia and Pennsylvania. As a regional crossroads town, just north of the Potomac River, Hagerstown was a favorite staging area for military leaders traversing the region.



*The Maryland Theatre*

A revitalized Arts & Entertainment District in downtown Hagerstown complements shopping, historical sites and museums in Washington County. Residents and visitors discover a wonderful collection of beautifully renovated retail shops, restaurants, and condominiums all within walking distance of cultural attractions. Hagerstown hosts several annual festivals downtown, most notably the Western Maryland Blues Fest, Augusterfest, and the Alsatia Mummer’s Day Parade.

## Form of Government

The County is a body corporate and politic, which performs all local governmental functions in Washington County except those performed by the nine incorporated municipalities within Washington County. The executive offices of the County are located at 100 West Washington Street, Hagerstown, Maryland 21740. The County’s central telephone number is (240)-313-2210 and its website is [www.washco-md.net](http://www.washco-md.net).

Under the Code of the Public Local Laws of Washington County (2019 Edition), as amended, being Article 22 of the Code of Public Local Laws of Maryland (the "County Code"), both the executive and legislative functions of the County are vested in the elected, five-member Board of County Commissioners of Washington County (the “Board”). The Board may only exercise such powers as are conferred upon it by the General Assembly of Maryland, including authorization to issue debt to finance its capital projects. Commissioners are elected on a countywide basis and serve four-year terms.

Each member of the Board has one vote and a simple majority of the Board is sufficient to take action subject to the authority vested in the Board by the County Code. Emergency action also requires a simple majority vote. The Board elects its own officers. The General Assembly of Maryland must authorize powers not specifically authorized by the County Code.

As authorized by the County Code, the County Commissioners appoint a County Administrator. The County Administrator is selected on the basis of his or her executive and administrative abilities, including his or her knowledge and experience in public administration. He or she is charged with the supervision of the departments and agencies of the County and is responsible for the day-to-day operations of the County government in conformity with public local laws and other laws applying to the County.

County financial matters are administered in part through the office of the Treasurer of Washington County. The County Code establishes the elective office of County Treasurer. The County Treasurer is constituted the collector of County and State taxes, charges and assessments and is charged with the enforcement of collection of taxes in the manner provided by law.

As authorized by the County Code, the Board appoints the Chief Financial Officer (CFO) on the basis of his or her experience in financial administration. The CFO is charged with assisting the Board in the preparation and administration of County budgets and other accounting and fiscal matters as the Board deems necessary. In addition, the CFO is responsible for the fiscal methods and procedures of each office, department, board, commission, institution, and agency of County government. The CFO reports to the County Administrator.

## Services

### Transportation

A variety of transportation avenues are available in Washington County. Hagerstown Regional Airport (HGR) is a Part 139 Facility which provides twice weekly service to Orlando Sanford International (SFB), via Allegiant. Allegiant also offers summer seasonal flights twice weekly to St Pete-Clearwater (PIE) and Myrtle Beach (MYR) airports. The airfield also offers fixed base operation services to general aviation, corporate and military aircraft. There are 17 businesses offering clients a variety of aviation services for all types of aircraft. Approximately 1,600 people are employed locally as a result of the airport being in Washington County. In addition, Dulles International, Baltimore Washington International Thurgood Marshall and Ronald Reagan Washington National airports are located within 70 miles of Hagerstown.

Other transportation outlets include auto rental services, County bus service, commercial bus lines, taxi, freight common carriers, and limousine service. The main lines of CSX and Norfolk Southern provide shipment to anywhere on the Atlantic Seaboard.

### Hospital and Medical Care

Meritus Health, located in Hagerstown, Maryland, is the largest healthcare provider in Western Maryland. As a community-focused, not-for profit system, Meritus Health's programs span the continuum of healthcare, ranging from inpatient care to occupational health services to physician practices and outpatient care.

Meritus Medical Center, which opened in 2010, is a state-of-the-art, Joint Commission-accredited hospital with 257 licensed beds in single-patient rooms. Services offered include a special care nursery, a level III trauma program, a primary stroke center, a wound center, and a cardiac diagnostic laboratory. Hospital services that address outpatient needs include the John R. Marsh Cancer Center, Total Rehab Care, Meritus Endocrinology Specialists, Meritus Home Health, Meritus Medical Laboratory and Equipped for Life.

Meritus Medical Group, with close to 100 physicians and advanced practice professionals, is a medical neighborhood of primary and specialty care practices offering comprehensive, coordinated health care services to all ages.

The Washington County Health Department, which provides various health services to the citizens of Washington County, employs a total of 146 full-time and part-time personnel in five divisions.

Other medical care facilities include the Environmental Health Division of the Health Department; the George W. Comstock Center for Public Health Research and Prevention; the Western Maryland Center, a State-owned chronic care facility; and the Brook Lane Psychiatric Center, a privately-owned psychiatric facility.

### Safety

The Washington County Sheriff's Office, the Maryland State Police, and municipal police agencies provide police protection in Washington County. The Sheriff's Office has 101 sworn personnel and 97 radio dispatched vehicles. The Sheriff's Office is responsible for the operation of the Detention Center, which has a capacity of 450 inmates. In October 2016, a Day Reporting Center opened that provides treatment services to non-violent offenders with drug and/or alcohol addictions. The State Police has 35 troopers assigned to the local barrack, which is located just south of Hagerstown. The Hagerstown Police Department has a full-time force of 110 officers. The Hancock Police Department employs four full-time officers. In addition, the Smithsburg Police Department employs four officers and the Boonsboro Police Department employs five police officers.

The County's Division of Emergency Services ("DES") oversees Emergency Communication/911, Emergency Management, Fire Department Special Operations, Fire Department Support Services, and the Emergency Medical Services Operations Program. DES is led by a full-time career director and four full-time department heads who oversee the daily operational components of Emergency Services in Washington County. The division has 104 full-time and part-time personnel working directly within the division and approximately 40 volunteers who provide dedicated service to the citizens of Washington County.

### Environmental Management

The Division of Environmental Management (“DEM”), which includes the Department of Water Quality, the Environmental Engineering Department, the Solid Waste Department, the Stormwater Management Department and the Watershed Department, was created in fiscal year 2007. The State and Federal environmental initiatives – as they pertain to water, wastewater, stormwater, solid waste and nutrients – are all jointly related. DEM is responsible for integrating the regulations and applying them to the operations of these departments.

The Washington County Solid Waste Department is responsible for a solid waste disposal system that protects the environment and public health.

The County currently provides water and/or wastewater services to nearly all of the immediate densely populated area surrounding the City of Hagerstown (except the Dual Highway corridor), the areas of Highfield, Elk Ridge, Sandy Hook, and the towns of Sharpsburg and Smithsburg.

### Department of Business Development

The Washington County Department of Business Development (the “DBD”) is dedicated to creating and sustaining a positive pro-business climate.

The DBD currently has six full-time employees to conduct the day-to-day operations of the office, as well as business development and business outreach efforts in the community.

Throughout the year the DBD meets with representatives of existing companies in need of assistance. Discussions include appropriate funding programs, incentive benefits, customized training programs, workforce development efforts, and other sources of business support. The DBD has formed strategic partnerships with such organizations as the Maryland Department of Commerce, the City of Hagerstown, the Washington County Chamber of Commerce, The Greater Hagerstown Committee, Inc., and Washington County Convention & Visitors Bureau, in order to better serve the needs of businesses in Washington County. The DBD was actively involved in Washington County’s becoming certified as the first ACT Work Ready Community in Maryland, an initiative that matches the labor force with available jobs in Washington County.

## *Demographic Information*

### Population

<u>Year</u>	<u>County Total</u>
1980	113,086
1990	121,393
2000	131,923
2010	147,430
2019	151,049
2025 projected	166,450
2030 projected	175,400
2035 projected	183,100

Sources: U.S. Census Bureau 1980, 1990, 2000, 2010, 2018; Projections by the Maryland Department of Planning 2017

### County Income

Per Capita Income	\$	30,222
Median Household Income	\$	63,126
Average Household Income	\$	79,183

Source: U.S. Census Bureau, 2018 American Community Survey

### Housing

#### 2018 Median Selling Price

Washington County	\$	190,088
Maryland	\$	293,930

Source: Maryland Association of Realtors.

### Households

Number of Households	56,306
Number of Family Households	37,072
Number of Non-family Households	19,234

Source: U.S. Census Bureau, 2018 American Community Survey

### Population Statistics

<b>Age:</b>	19 & under	24.20%
	20-64 (workforce age)	59.30%
	65 & older	16.50%
	Median Age	40.5
<b>Gender:</b>	Male	50.80%
	Female	49.20%
<b>Race:</b>	White	82.70%
	Black	10.50%
	Other	6.80%

Source: U.S. Census Bureau, 2014-2018 American Community Survey

### Education Facilities in Washington County

#### Higher Education:

Hagerstown Community College  
Purdue University Global - Hagerstown  
University System of MD at Hagerstown

#### Primary Education:

26 Elementary Schools  
7 Middle Schools  
9 Senior High Schools  
1 Middle/Senior High School  
1 Technical High School  
1 Evening High School  
1 Outdoor Education  
1 Special Education  
Pupil/Teacher Ratio: 22:1  
Public Enrollment: 21,830  
39 Private Schools

Source: Washington County Board of Education



**County Water Quality Systems**

	<u>Total # of Services</u>
Full Service Water	1,350
Full Service Sewer	7,313
Collection Service Sewer	3,834
Total	12,470

Source: Washington County Department of Budget and Finance

**County Building Permits (000s)**

	<u>Number</u>	<u>Value</u>
Residential New	200	\$50,455
Other Permits	1,002	\$175,029
Total	1,202	\$225,484

Source: Washington County Department of Permitting

**County Employment Statistics**

Civilian Labor Force	74,846
Employed	71,872
Unemployed	2,974
Unemployment Rate	4.0%
State Average	3.5%

Source: MD Department of Labor, Licensing &amp; Regulation March 2020

**Top 15 Employers in Washington County**

	<u>Employer</u>	<u>Employment</u>
1	Washington County Public Schools	3,396
2	Meritus Health, Inc.	2,740
3	Citi	2,300
4	Fiserv	2,185
5	State of Maryland	2,030
6	Washington County Government	1,401
7	Volvo Group	1,300
8	FedEx Ground	900
9	Bowman Group, LLP	745
10	Federal Government	582
11	Hagerstown Community College	545
12	Merkle Response Services, Inc.	545
13	ARC of Washington County	500
14	Direct Mail Processors	500
15	City of Hagerstown	486

Source: Maryland Department of Commerce

**County Business Patterns**

<u>Industry</u>	<u>Total # Establishments</u>
Services	1,266
Retail Trade	601
Other	404
Construction	335
Finance, Insurance, Real Estate	335
Transportation/Warehousing	151
Wholesale Trade	157
Manufacturing	127
Information	47
Utilities	4
Mining	2
Agricultural	4
Total	3,433

Source: U.S. Census Bureau, 2017 County Business Patterns

**Top 20 Largest Taxpayers in Washington County  
as of June 30, 2019  
Ranked by Assessed Value**

	<u><b>Taxpayer</b></u>		<u><b>Assessed Value</b></u>
1	PR Valley Limited Ptsp	\$	112,138,050
2	Outlet Village of Hagerstown		101,847,233
3	Potomac Edison		94,886,420
4	Bowman Group		87,731,984
5	Liberty Property Limited		71,819,633
6	Walmart Stores/Wal-Mart R.E./Sam's R.E./Sam's East		62,855,723
7	FedEx Ground Packaging System		60,604,210
8	CR Hagerstown LTD		56,375,700
9	LCN STP Hagerstown LLC		48,819,990
10	Ghattas Enterprise Maugans Ave.		47,156,577
11	Western Hagerstown (Inc. & Dist.)		46,094,500
12	2007 East Greencastle Pike		44,934,600
13	254 Hagerstown/Citigroup/Citicorp		43,765,667
14	Mack Truck Inc./Volvo Group		35,070,680
15	Intelsat Global Service LLC		34,140,940
16	Verizon		34,071,650
17	Norfolk Southern Combined Rail		33,737,100
18	Columbia Gas of Maryland		33,690,247
19	Cortpark II LLC		32,936,300
20	Lowe's Home Centers Inc		32,149,940

Source: Washington County Treasurer's Office

The information set forth above was compiled from tax rolls on which the names and owners are not always recorded in the same way.

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# Capital Improvement Plan Development

## *Capital Improvement Program*

Washington County government is responsible for providing infrastructure improvements to its citizens. To provide for these improvements on a continuing basis, the Board of County Commissioners established a Capital Improvement Program that forecasts the future needs and priorities of the community. Through sound planning and programming of capital projects, the County can provide many improvements while utilizing the prescribed amount of funds available for this purpose.

A major purpose of the Capital Improvement Program is to provide a means for coordinating and consolidating all departmental and agency project requests into one document. The projects can then be examined and prioritized based on established criteria that includes County plans and policies.

A Ten-Year Capital Improvement Plan is developed each fiscal year and includes scheduling and financing of future community facilities such as public buildings, roads, bridges, parks, water and sewer projects, and educational facilities. The plan is flexible and covers ten years with the first year being the Capital Improvement Budget. Funds for each project are allocated from Federal, State, and local sources by the County Commissioners.

The Capital Improvement Plan not only accounts for the acquisition, expansion, and rehabilitation of infrastructure and other capital assets, but it also incorporates the following basic underlying principles of the County:

- Capital projects are targeted to support the goals of the Comprehensive Plan and other County functional plans.
- Capital investments are made for economic development.
- Existing assets are preserved and maintained to ensure continued service.
- External funding possibilities are considered when reviewing and prioritizing projects.
- Intergovernmental funding is sought for regional projects.
- Debt is used carefully and managed in accordance with the goals and objectives of County policies.

The Ten-Year Capital Improvement Plan is updated on an annual basis for both project costs and funding sources. Along with the development of the Capital Improvement Plan, an annual debt affordability analysis is performed to evaluate the effect of debt service costs on operating budgets and to utilize long-term financial planning. As part of the annual review process, certain projects are closed out due to completion and others are added as the re-sequencing of project priorities occur.

## *Capital Improvement Plan Process*

Capital Improvement Plan development begins in the winter after the completion of the debt affordability analysis. The Capital Improvement Program provides a comprehensive approach to planning and impacts all facets of County operations. The County Administrator, the Chief Financial Officer, the Planning Director, the Director of Public Works, and the Director of Engineering comprise the Capital Improvement Plan Committee, ("CIP Committee"). From the time the Capital Improvement Plan's initial annual review begins in October through its adoption in May of each year, there is constant interaction between departments, the CIP Committee, and the elected officials. This effort is characterized by cooperation and reflects a common goal of ensuring that the Capital Improvement Plan meets the objectives of the County and remains affordable and achievable. The CIP Committee reviews the project submissions to ensure:

- The plans are properly coordinated with other projects, entities, etc.;
- Long-term operating impacts are included in estimates (including staffing, utility, maintenance, and debt);
- Timeframes for construction activity and cash flow are realistic;
- The budget and appropriate funding sources are adequate; and
- Projects are prioritized based on County goals, department priorities, and anticipated funding sources.

The Capital Improvement Plan is reviewed in conjunction with the debt affordability analysis and revenue projections, inclusive of rate analysis, in order to determine funding availability. A financial analysis of funding sources and project costs is conducted for all proposed capital improvement projects. It is the CIP Committee's responsibility to review all requests that County departments and agencies submit. All projects are ranked based on established criteria for priority ranking. Considering current and future needs, as developed in the ten-year plan, available funding sources, and the results of the priority ranking process, the CIP Committee determines which capital projects best meet established criteria for the current fiscal year Capital Improvement Budget and the nine-year forecast.

The Board of County Commissioners reviews the Ten-Year Capital Improvement Plan in regular public working sessions and at the public hearing. Following this review and before the end of the fiscal year, the Board formally approves and adopts the Ten-Year Capital Improvement Plan for the established projects.

## *Components of the Capital Program*

### Capital Project Definition

Capital Projects are included in the Capital Improvement Plan when the project supports or improves infrastructure needs and/or the productive capacity of the County. Projects should have a useful life greater than five years and an estimated cost of \$10,000 or more, and should also meet one or more of the following criteria:

- Projects having restricted funding sources, including grants, that require them to be included in the Capital Improvement Budget;
- Systematic acquisitions over an extended time period to complete implementation of a major functional or operating system;
- Rehabilitation or replacement projects of governmental or agency facilities;
- Projects that require bond financing because of significant costs associated with acquisition or construction of the project; and
- Planning and feasibility studies that support the acquisition, construction, or improvement of the items listed above. (These are not required to meet the useful life test).

Capital assets include land, improvements to land, easements, buildings, building improvements, vehicles, machinery, large equipment, infrastructure, and all other tangible and intangible assets that are used in operations. Assets not meeting the criteria above are budgeted as capital outlay in the Operating Budget.

### Capital Project Priority Ranking System

During the review process the CIP Committee prioritizes projects based on the County's broad goals, department priorities, anticipated funding sources, and the priority-ranking matrix. The priority-ranking matrix is composed of 14 scored and weighted criteria, which is the basis for assigning projects into one of the five priority-ranking categories.

The 14 scored and weighted ranking criteria used by Washington County are:

1. Legal Mandates – This criterion assesses the risk with legal issues required by Federal or State statute, court order, or regulation, or a project that moves the County into further compliance with such mandates.
2. Public Health and Safety – This criterion includes health related impacts such as increases in traffic accidents, injuries, and deaths.
3. Environmental Impact – This criterion evaluates the environmental related impact on items such as water quality, flood control, air quality, contamination, etc.

4. **Conformity to County Commissioners Goals and Plans** – This allows for the evaluation of the project in relationship to the goals and plans of the Commissioners and/or the Comprehensive Plan or other approved plans by the County.
5. **Conformity to Agency, Department and Jurisdictional Plans** – This allows for the evaluation of the project in relationship to written plans of County agencies, departments, and jurisdictions.
6. **Community Support** – This criterion refers to interest group advocacy and/or opposition and conformity to County master and strategic plans.
7. **Project Cost** – This criterion considers the total cost of constructing or installing the proposed work. The higher the cost the lower the weight. However, the forced score should not be considered adversely with respect to an individual project as it simply identifies the financial issues within the Capital Improvement Plan Budget. A project will rank high if warranted by other evaluation criteria when scored if appropriate.
8. **Funding** – This criterion evaluates available funding sources to be contributed towards the proposed project, taking into consideration if the project is continuing from the prior year (in which funding would be required), if there is proposed self-supporting funds, or if significant outside funding sources are available.
9. **Operating Budget Impact: Cost/Benefit** – This criterion reflects other costs relative to the proposed project, including operation and maintenance, start-up costs, and personnel, as well as cost savings and potential revenues generated by the completed project.
10. **Preservation of Facility** – Measures the possible effect of deferring the project, such as complete replacement of facility or equipment, major repair, normal repair costs if not replaced or added cost for new facility.
11. **Project Life** – This criterion is used to rank the project based on life expectancy and projected maintenance cost. A higher value is placed on longer-term infrastructure assets.
12. **Economic Impact** – Measures the impact such as property value, future tax base, added jobs, income to citizens, changes in business income, and stabilization of neighborhoods. Such impacts may apply more to capital projects related to growth and expansion than to infrastructure maintenance, although deteriorating structures can adversely affect business.
13. **Recreational, Cultural or Aesthetic Value** – A catch-all criteria for other significant quality-of-life related impacts that include community appearance, recreational opportunities, and cultural improvements.
14. **Percent of Population Benefiting** – Estimates the number of persons likely to be affected by the project and nature of the impact.

After the projects are scored and weighted using the above criteria, the projects are then categorized based on the score into Priority 1 through Priority 5. The priority category definitions are as follows:

- |            |   |
|------------|---|
| Priority 1 | Projects needed to comply with a court order or legislative mandate, and/or projects that are critical to the health, safety, and general welfare of County citizens. |
| Priority 2 | Projects essential to the general welfare of the community, operating or maintaining of a physical facility, but not critical relative to other projects.             |
| Priority 3 | Projects that provide a public operational improvement or are important in relation to County financial capabilities, needs, or other program requirements.           |
| Priority 4 | Projects that provide for necessary maintenance or replacement, but where deferral will not result in significantly increased cost to the County.                     |



- Priority 5 Projects that conflict with the master plan and/or projects for which there are concerns related to serious need, cost, justification, or timing.

After all proposed projects are prioritized using these criteria, the CIP Committee (1) reviews the project ranking report for reasonableness; (2) checks for any projects that appear out of order; (3) determines if there are any linkages between projects; (4) evaluates if there are any advantages to having projects done concurrently; (5) ascertains if there are any projects dependent on one another; and (6) reviews the project's impact on the operating budget. Adjustments to the final ranking may be necessary based on this extensive Capital Improvement Plan prioritization process.

The CIP Committee provides an enterprise-wide view and prioritizes proposed projects while balancing project requests against known County objectives. After their review is complete, the CIP Committee presents the Ten-Year Capital Improvement Plan to the Board of County Commissioners. The County Commissioners review the recommended Ten-Year Capital Improvement Plan during budget workshops and at a public hearing prior to budget adoption.

Program priorities, long-term service needs, and planning – like multidimensional ranking systems – complement rather than replace, the judgment that County officials must exercise in ranking requests for capital projects and acquisitions and developing the Capital Improvement Plan and Capital Budget.

### Capital Improvement Plan Funding Sources

Funding sources in the Capital Improvement Plan are budgeted in each fiscal year in which they will be received. All potential funding sources are considered when developing the Capital Improvement Plan to ensure that projects are funded with appropriate revenue streams. Some projects are funded with project-specific revenues that benefit particular projects and/or users, as is the case with development fees. The other information considered when funding the Capital Improvement Plan are the results of the Debt Affordability Analysis, cash flow requirements of each project, and the financial costs associated with each funding source. Following are descriptions of the funding sources for the Capital Improvement Budget:

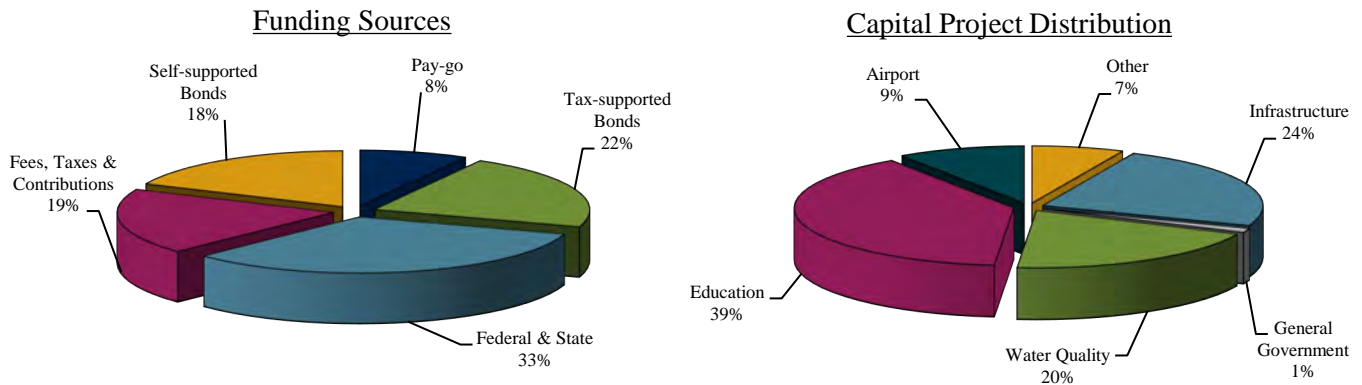
- Tax-Supported Bonds are General Obligation Bonds secured by the full faith and credit of the issuer. General Obligation Bonds issued by the County are secured by a pledge of the County's property taxing power, and must be authorized by legislative authority. The amount to be issued in this fiscal year is based on the County's debt affordability guidelines, debt policies, and future project costs.
- Self-Supported Bonds are issued for enterprise funds and the debt service is paid from user fees.
- Pay-Go Funds represent cash contributions from various operating funds for specific capital projects without a dedicated funding source.
- Federal and State Funds are for specific projects and are restrictive in nature. These funds are inconsistent from year-to-year and are not used as a base revenue stream.
- Fees and Taxes is the other major funding source. Excise tax is assessed for residential construction at \$1 per sq foot (SF), and \$.50 SF for additions to residential properties. Excise tax for nonresidential non-retail is assessed at \$1 SF and nonresidential retail at \$1 SF for the first 15,000 SF and \$3 SF thereafter. Budgeted excise tax is based on projected growth and development. Transfer tax is charged on recorded real property transfers in the County at a rate of .5%. Transfer tax revenue is based on projected home sales and recordation activity. In addition, the County has an Adequate Public Facilities Ordinance Fee and is used to generate revenue so that public facilities and services needed to support new development will be available concurrently with the impacts of the new developments.

# Fiscal Year 2021 Overview

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## *FY21 Capital Improvement Budget Overview*

A graphic illustration of planned capital project distribution and funding sources for fiscal year 2021 is shown below. The fiscal year 2021 Capital Improvement Budget is funded mainly from: tax-supported and self-supported bonds, pay-go funds, Federal and State funds, and developer-based fees.



## *Operating Impact of Capital Improvement Projects*

The operating impact of capital projects are reviewed and considered during the Capital Improvement Plan review process. Estimated new revenues and or operational efficiency savings associated with projects are also taken into consideration. The operating costs of the projects are identified and considered during the ranking evaluation. The operating costs of projects to be completed and in-service during the budget year are identified and justified as part of the operating budget process. Additions or eliminations of personnel, significant start-up costs, as well as operation and maintenance costs of the new facilities are included in the operating budget. These conservative operating cost estimates include: personnel, equipment maintenance, building maintenance, and other major costs anticipated at the completion of the project. An important consideration in the County's Capital Improvement Budget is the fiscal impact on the Operating Budget due to the additional debt service cost. Total debt service cost for fiscal 2021 is approximately \$20.2 million.

The ten-year operating budget impact for major projects approved in the fiscal year 2021 Capital Budget are presented in the project detail schedule located in the next section.

## *Debt Affordability Analysis*

One of the major sources of funding for capital projects is debt. Debt is issued to match the cash flow requirements of the Capital Improvement Plan, while considering the effect of the tax burden on the citizens. Therefore, along with the development of the Capital Improvement Plan, an annual debt affordability analysis is performed to evaluate the effect of debt service costs on operating budgets and to utilize long-term financial planning.

The key factor in the issuance of debt is the understanding that effective debt management is critical to the overall financial management of the County. Therefore, the issuance of debt is done in conformance with the County's Debt Policy, which is an integral component of the County's financial management program. Accordingly, critical to sound financial and debt management is the continuing evaluation of the County's ability to afford and plan for the issuance of debt. In this regard the County has instituted a self-imposed requirement that an annual debt affordability analysis be performed. The analysis provides a method by which the County's debt position can be evaluated, tested for stress and affordability, and compared to other jurisdictions, "Peer Group", that are considered comparable to the County. The analysis provides the County Commissioners and citizens with a way to assess the impact of bond issuance and allows for informed decisions regarding financing proposals and capital spending priorities.

The County undertakes the debt capacity analysis on an annual basis in conjunction with the issuance of bonds and the formulation of the long-range Capital Improvement Plan. This comprehensive and routine analysis of debt capacity provides assurance that the amount of debt issued by the County is affordable. It also ensures that an appropriate balance is maintained between the County's capital needs and its ability to pay for them.

To determine the County's debt affordability, a peer group was established for comparison. The Peer Group consists of counties that share similar characteristics in various areas, including but not limited to, population, region, bond rating, and budget thresholds. Also, our sample of counties was based on size and income indicators, such as per capita income, property values, access to interstate highways, and revenue generation.

The Peer Group contains nine other Maryland counties: Howard, Frederick, Harford, Carroll, Charles, St. Mary's, Cecil, Wicomico, and Calvert.

As with any business, including County government, it is important to develop strategic objectives, including prudent borrowing limits. The debt ratios used by the County are relevant benchmarks used to measure its debt position. Establishing an acceptable range for the debt ratios has allowed the County to continually monitor its debt position and provide a mechanism for calculating debt capacity. The information provided by the ratios assists the County in the capital budgeting decision process, including prioritizing capital spending.

Measures of debt affordability are sensitive as they are impacted by the amount of outstanding debt and changes in both demographic and economic factors. Changes in demographic factors such as population growth and personal income affect debt ratios. Economic cycles can have major impacts, both positive and negative, on targeted ratios and debt capacity. This volatility demonstrates the need for assessing changes in projected debt capacity on an annual basis.

Decisions regarding the use of debt is based on a number of factors including, but not limited to, the long-term needs of the County and the amount of resources available to repay the debt. Flexibility is required to enable the County's management team to respond to unforeseen emergencies or opportunities in the operational budget. In order to provide for that flexibility, the most important ratio - Debt Service as a Percent of Revenue - is included in the analysis. Comparing debt ratios of the Peer Group and national medians is useful in evaluating the County's debt position. Evaluating the change in ranking over time also indicates a strengthening or weakening of the County's debt position relative to the Peer Group and to national averages. Following is a five-year comparison of the County's debt ratios for the tax-supported debt portion.

Ratio and Peer Group Median Comparisons								
Fiscal Year	Debt Per Capita		Debt as a Percent of FMV		Debt Service as a % of General Fund Revenue		Debt Service per Capita as a % of Income Per Capita	
	County Amount	Peer Group Median	County Ratio	Peer Group Median	County Ratio	Peer Group Median	County Ratio	Peer Group Median
2016	933	1,661	1.16%	1.68%	6.36%	8.06%	.22%	.32%
2017	939	1,573	1.16%	1.56%	6.67%	8.98%	.22%	.33%
2018	952	1,614	1.17%	1.64%	6.59%	8.94%	.21%	.34%
2019	967	1,646	1.16%	1.50%	5.91%	9.32%	.20%	.34%
2020 estimated	1,069	1,646	1.28%	1.50%	6.35%	9.32%	.20%	.34%
Policy		1,500		1.50%		8.00%		0.50%

When the County compares its debt ratios to its peer group and national medians, it provides a snapshot of our debt position at a single point in time. However, to fully understand the County's debt position, it is important to evaluate ratios over a long period of time so that trends can be ascertained, analyzed, and evaluated.

In completing the debt affordability analysis the estimated debt capacity ceiling is established and policy guidelines are applied to the debt capacity calculations. The ratio of Debt Service as a Percentage of Revenue is considered the most critical criteria in establishing debt capacity, in part, because the County controls both components of the ratio and the impact of the change is most pronounced in the operating budget and potentially the tax burden carried by the citizens.

Projections are based on net tax-supported debt currently outstanding plus average debt that is anticipated to be issued over the next 20 years. The projections are intended only to provide a method for assessing the impact of issuing more debt. The County's debt affordability analysis is designed to: ensure that anticipated future debt is manageable from a fiscal and budgetary perspective; meet peer group ratio targets and avoid negative treatment by the rating agencies in the form of a rating downgrade; and keep borrowing costs to a minimum. The following table illustrates the impact of long-term debt issuance as it relates to various Peer Group targets that the County monitors.

Debt Capacity Analysis – Effect of Debt Issuance on Debt Ratios								
Fiscal Year	Debt Per Capita		Debt as a Percent of FMV		Debt Service as a % of General Fund Revenue		Debt Service Per Capita as a % of Income Per Capita	
	Projected	Peer Group Median	Projected	Peer Group Median	Projected	Peer Group Median	Projected	Peer Group Median
2021	1,107	1,646	1.32%	1.50%	6.99%	9.32%	0.22%	0.34%
2022	1,132	1,646	1.34%	1.50%	6.86%	9.32%	0.22%	0.34%
2023	1,158	1,646	1.36%	1.50%	7.07%	9.32%	0.22%	0.34%
2024	1,180	1,646	1.37%	1.50%	7.31%	9.32%	0.23%	0.34%
2025	1,196	1,646	1.38%	1.50%	7.50%	9.32%	0.24%	0.34%
2026	1,208	1,646	1.38%	1.50%	7.52%	9.32%	0.24%	0.34%
2027	1,219	1,646	1.38%	1.50%	7.56%	9.32%	0.24%	0.34%
2028	1,227	1,646	1.38%	1.50%	7.77%	9.32%	0.24%	0.34%
Policy		1,500		1.50%		8.00%		0.50%

Debt Capacity Analysis – Effect of Debt Issuance on Debt Ratios								
Fiscal Year	Debt Per Capita		Debt as a Percent of FMV		Debt Service as a % of General Fund Revenue		Debt Service Per Capita as a % of Income Per Capita	
	Projected	Peer Group Median	Projected	Peer Group Median	Projected	Peer Group Median	Projected	Peer Group Median
2029	1,230	1,646	1.37%	1.50%	7.22%	9.32%	0.23%	0.34%
2030	1,241	1,646	1.37%	1.50%	7.24%	9.32%	0.23%	0.34%
2031	1,250	1,646	1.37%	1.50%	7.54%	9.32%	0.24%	0.34%
2032	1,253	1,646	1.36%	1.50%	7.54%	9.32%	0.24%	0.34%
2033	1,253	1,646	1.35%	1.50%	7.56%	9.32%	0.24%	0.34%
2034	1,251	1,646	1.34%	1.50%	7.56%	9.32%	0.24%	0.34%
2035	1,248	1,646	1.32%	1.50%	7.57%	9.32%	0.24%	0.34%
2036	1,242	1,646	1.30%	1.50%	7.50%	9.32%	0.23%	0.34%
2037	1,236	1,646	1.29%	1.50%	7.47%	9.32%	0.23%	0.34%
2038	1,227	1,646	1.27%	1.50%	7.47%	9.32%	0.23%	0.34%
2039	1,217	1,646	1.25%	1.50%	7.45%	9.32%	0.23%	0.34%
2040	1,205	1,646	1.22%	1.50%	7.43%	9.32%	0.23%	0.34%
Policy		1,500		1.50%		8.00%		0.50%

### Credit Ratings

Rating agencies are companies that assign credit ratings to institutions, including local governments that issue debt obligations. Credit ratings are the rating agencies' assessment of the County's ability and willingness to repay debt on a timely basis. Debt management is an important factor in evaluating and assigning credit ratings. Credit ratings are an important indicator in the bond market and can influence the County's long-term interest rates that it must pay.

The County's current credit ratings are AA+ by Standard & Poor's, AA+ by Fitch and an Aa1 by Moody's Investors Service. The County's credit ratings reflect strong financial management, stable and growing property tax base, strong budgetary performance, strong financial policies, low debt burden, and sound reserve levels.

### Changes in Economic Assumptions

In addition to analyzing the impact of the capital program on debt ratios and capacity, to remain prudent, the County analyzes the impact of changing economic conditions on the recommended maximum level of annual debt issuance. Three economic scenarios are created:

- 'Base' case reflects future economic conditions based on historical and projected trends.
- 'Best' case reflects the best economic conditions based on historical high trends.
- 'Worst' case reflects the worst economic conditions based on historical low trends.

The assumptions used in determining debt capacity in each scenario are based on historical trends, judgment, and projected economic conditions. Each case assumption is applied to the 20-year projection.

The 'Base' case projects the most affordable program. The 'Best' and 'Worst' case scenarios assume major changes in economic conditions for the 20-year period and could require adjustments to the Capital Improvement Plan and the debt issuance plan. However, planning to issue debt on the 'Best' case scenario every year is not advisable because some bonding capacity should be kept in reserve in anticipation of sudden unexpected economic downturns.

## Final Analysis

There are multiple factors that can affect the County's affordability to incur future indebtedness, including the County's economy and the availability of adequate financial resources. For that reason, the financial ratios and analysis used, take into account the entire County financial condition, as other factors can effectively deteriorate the County's financial posture and affect its ability to incur debt. In addition, these managerial and unpredictable scenarios are considered and tested as part of the analysis, so that the known effects of 'Worst' and 'Best' case results can be examined. It is important for the County to monitor its financial condition, economic trends, and debt affordability results on a regular basis, in order to continue to evaluate the County's credit position to determine whether annual issuance of debt should be adjusted to reflect a changing financial outlook for the County under altered circumstances.

The estimated maximum debt capacity is not intended to be an absolute limit or a recommendation on the amount of debt that can be incurred. It should be used as a guide for better long-term financial planning and improving capital budgeting. Debt capacity estimates can assist long-term capital planning by showing the resources available to fund needed infrastructure, schools and other capital needs. The estimates can then be used to allocate restricted resources to priority projects. The County's annual debt review analyzes the projected debt issuances to assure that long-term financial stability will remain intact.

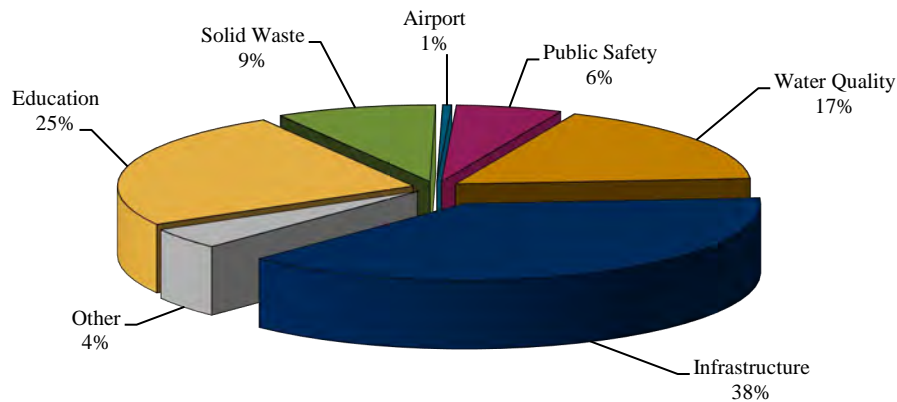


## *Outstanding Debt*

The impact of the Capital Improvement Budget on the Operating Budget is evaluated during the budget process. The Capital Improvement Budget includes costs for long-term capital projects, which add to the capital asset base of the County. The Operating Budget includes the principal and interest cost associated with those assets and the pay-go financing.

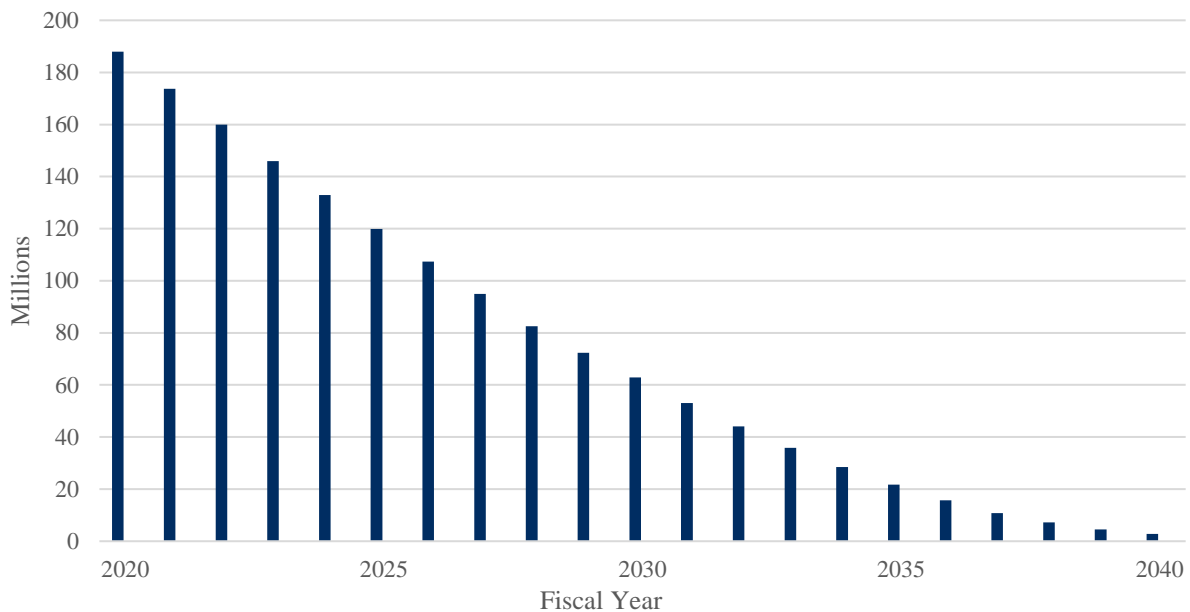
The following graph illustrates the percentage of total debt outstanding at June 30, 2020, by project category:

Percentage of Debt Outstanding by Project Category



The following graph illustrates the current outstanding debt at June 30, 2020.

Outstanding Debt



The table below shows the current outstanding balance at June 30, 2020, and the estimated impact of debt service on the Operating Budget for FY 2021.

### Current Debt Balance and FY 2021 Principal and Interest Costs

Description	FY 2020 Balance	FY 2021 Principal	FY 2021 Interest	Total Debt Service
<b>General Fund:</b>				
2010 Ser A Public Improvement Bonds	\$ 564,090	\$ 564,090	\$ 8,462	\$ 572,552
2010 Refunding Bonds	1,734,536	1,322,571	32,197	1,354,768
2011 Public Improvement Bonds	7,387,512	506,153	264,423	770,576
2012 Public Improvement Bonds	8,702,075	546,852	255,280	802,131
2012 Refunding Bonds	1,828,700	784,360	73,140	857,500
2013 Public Improvement Bonds	9,125,000	535,000	279,550	814,550
2013 Refunding Bonds	7,789,232	1,295,638	240,642	1,536,280
2014 Public Improvement Bonds	11,475,000	570,000	431,938	1,001,938
2015 Public Improvement Bonds	10,304,010	469,599	377,600	847,199
2015 Refunding Bonds	15,886,092	1,730,015	600,843	2,330,858
2016 Public Improvement Bonds	10,708,990	468,137	345,091	813,228
2016 Refunding Bonds	6,217,650	575,082	208,657	783,739
2017 Public Improvement Bonds	11,211,901	426,705	400,911	827,617
2018 Public Improvement Bonds	11,623,058	393,511	447,240	840,751
2019 Public Improvement Bonds	12,000,000	369,647	417,363	787,010
2020 Public Improvement Bonds	9,032,740	0	152,238	152,238
2020 Refunding Bonds (2010 BABs)	5,755,630	0	150,286	150,286
MD Water Quality Solid Waste Refinancing	524,045	5,337	5,240	10,578
MD Water Quality Resh Capping Ph 1	1,952,527	270,675	19,526	290,201
Total General Fund Existing Debt	\$ 143,822,788	\$ 10,833,371	\$ 4,710,629	\$ 15,544,000
2021 Planned Debt:	12,000,000			
2021 Public Improvement Bonds				
Total General Fund Debt	\$ 155,822,788	\$ 10,833,371	\$ 4,710,629	\$ 15,544,000
<b>Solid Waste:</b>				
2010 Ser A Public Improvement Bonds	223,289	223,289	3,349	226,638
2010 Refunding Bonds	410,464	407,429	6,203	413,632
2011 Public Improvement Bonds	2,172,488	148,847	77,760	226,608
2012 Refunding Bonds	5,640	5,640	230	5,870
2013 Refunding Bonds	105,768	19,362	3,358	22,720
2015 Refunding Bonds	1,340,851	148,983	50,654	199,637
2016 Public Improvement Bonds	91,919	4,018	2,962	6,980
2016 Refunding Bonds	895,235	82,802	30,043	112,845
2017 Public Improvement Bonds	1,066,999	40,608	38,153	78,762
2018 Public Improvement Bonds	825,237	27,939	31,754	59,693
2019 Public Improvement Bonds	255,000	7,855	8,869	16,724
2020 Public Improvement Bonds	27,260	0	459	459
2020 Refunding Bonds (2010 BABs)	2,278,300	0	59,489	59,489

Current Debt Balance and FY 2021 Principal and Interest Costs

Description	FY 2020 Balance	FY 2021 Principal	FY 2021 Interest	Total Debt Service
MD Water Quality 40 West Cell 3	671,413	141,445	7,386	148,830
MD Water Quality Solid Waste Refinancing	1,439,716	14,663	14,397	29,060
Total Solid Waste Existing Debt	\$ 11,809,580	\$ 1,272,880	\$ 335,070	\$ 1,607,950
2020 Planned Debt: 2020 Public Improvement Bonds	190,000			
Total Solid Waste Debt	\$ 11,999,580	\$ 1,272,880	\$ 335,070	\$ 1,607,950

**Airport:**

2012 Refunding Bonds	375,660	100,000	15,030	115,030
Total Airport Existing Debt	\$ 375,660	\$ 100,000	\$ 15,030	\$ 115,030

**Water Quality:**

2010 Ser A Public Improvement Bonds	97,621	97,621	1,464	99,085
2012 Public Improvement Bonds	4,107,925	258,148	120,508	378,656
2015 Public Improvement Bonds	2,970,990	135,401	108,875	244,276
2015 Refunding Bonds	708,057	76,003	26,802	102,805
2016 Public Improvement Bonds	7,614,092	332,845	245,360	578,205
2016 Refunding Bonds	2,077,115	192,116	69,706	261,822
2017 Public Improvement Bonds	596,099	22,687	21,315	44,002
2018 Public Improvement Bonds	1,581,705	53,550	60,862	114,412
2019 Public Improvement Bonds	1,055,000	32,498	36,693	69,191
2020 Public Improvement Bonds	5,090,000	0	77,028	77,028
2020 Refunding Bonds (2010 BABs)	996,070	0	26,008	26,008
MD Water Quality Series BNR	209,238	209,238	3,557	212,795
MD Water Quality Pretreat. Refinancing 2004	1,366,063	445,000	5,464	450,464
MD Water Quality Halfway I & I	208,795	28,945	2,088	31,033
MD Water Quality Winebrenner	2,074,419	122,048	16,596	138,644
MD Water Quality Conococheague	1,257,066	65,773	8,799	74,573
Total Water Quality Existing Debt	\$ 32,010,255	\$ 2,071,873	\$ 831,125	\$ 2,902,999
2020 Planned Debt: 2020 Public Improvement Bonds	9,820,000			
Total Water Quality Debt	\$ 41,830,255	\$ 2,071,873	\$ 831,125	\$ 2,902,999
Total Existing and 2021 Planned Debt	\$ 210,028,283	\$ 14,278,124	\$ 5,891,855	\$ 20,169,979

Bonded Limit Summary as of June 30, 2020

The County may only issue general obligation and revenue bonds under authority conferred by the Maryland General Assembly, excluding those issued for education. No referendum is required. As of June 30, 2020, the unused authorization available for issuance of general obligation bonds was \$59,515,722.

By State law, the total bonded indebtedness of the County for Water, Wastewater, and Pretreatment purposes may not exceed 10% of the assessed value of all property in Washington County subject to unlimited County taxation. See following table.

Schedule of Legal Debt Margins – Department of Water Quality Estimated as of June 30, 2020	
Assessed Value of Property in Washington County	\$13,417,811,000
Debt Limit: % of Assessed Value	10%
Water Quality Borrowing Limitation	1,341,781,100
Water Quality Debt	32,010,255
Debt Margin	1,309,770,845
Ratio of Water Quality Debt to Assessed Value	0.24%

*Statement of Revenues and Expenditures*  
*Summary By Year – Capital Improvement Fund*

Description	Fiscal Year		
	2019 Actual	2020 Budget	2021 Budget

Revenue (By Major Type):

Fees	\$ 4,541,145	\$ 2,900,000	\$ 2,600,000
Grants	7,472,875	28,429,000	17,962,000
Other	39,141	3,483,000	7,836,000
Bonds	14,221,017	16,669,000	22,010,000
Subtotal	26,274,178	51,481,000	50,408,000
Transfers	7,545,330	2,413,000	1,929,000
Capital Reserves	0	2,425,000	2,537,000
Total Revenue	33,819,508	56,319,000	54,874,000

Expenditures (By Function):

Education	1,295,680	26,057,000	21,503,000
Public Safety	1,576,820	1,956,000	2,603,000
Court System	51,050	0	0
General Government	5,303,451	1,036,000	609,000
Parks and Recreation	94,929	448,000	574,000
Water Quality	2,765,690	6,715,000	10,384,000
Roads/Infrastructure	12,828,155	9,612,000	13,122,000
Solid Waste	118,597	527,000	373,000
Transit System	626,729	1,353,000	545,000
Airport	2,220,676	8,615,000	5,161,000
Golf Course	82,568	0	0
Total Expenditures	26,964,345	56,319,000	54,874,000

Net Difference	1,347,199	0	0
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*Project Detail of Major Projects – Fiscal Year 2021*

Project Name	Project Description	Project Budget	Operating Impact
Capital Maintenance - BOE	Projects vary depending on the conditions, safety, security, and utility requirements. The Comprehensive Maintenance Plan outlines specific projects over the next five years. Projects which qualify for State funding of 79% of construction costs are included here. Projects are targeted to reduce deferred maintenance.	\$6,701,000	\$0
Capacity Management Project	Capacity Management has been expanded to include the following: Phase I – Construct a new regional pump station to service the Maugansville area. The new pump station will pump the sewage to the Cedar Lawn area where it will be discharged into the County's gravity collection system. Gravity lines will be constructed from the new station to Maugansville and Maugans Meadow's pump stations. Once the new station is on-line the existing Maugansville and Maugans Meadow's pump stations will be taken off-line. Phase II – Construct a new gravity line from the regional station to the Garden Spot pump station. At this time, the Garden Spot pump station will be taken off-line. Phase III – Construct a new gravity line from Garden Spot pump station to Freedom Hills pump station. At this time, the Freedom Hill pump station will be taken off-line.	6,440,000	0
Sharpsburg Elementary School Replacement	The project involves construction of 60,054 SF replacement building to support 473 students.	6,191,000	0
Center for Business and Entrepreneurial Studies	The project includes an extensive first floor renovation along with major building upgrades to create the Center for Business and Entrepreneurial Studies. The current building has large areas of space that was constructed for training in light manufacturing disciplines. As the market has changed, so has the demand for this type of space. The project will make these areas suitable for other purposes, including the creation of a business incubator. The renovation will create a lobby area, modern lab areas, classrooms and shared spaces for business incubation areas.	4,449,000	0

Project Name	Project Description	Project Budget	Operating Impact
Pavement Maintenance and Rehab Program	This project includes the modification of county-wide pavement maintenance program targeting rehabilitation of county highway pavement, as required. Techniques may include but not be limited to road reclamation, bituminous concrete overlay, crack sealing, and surface treatment. Individual projects will be determined on an annual basis consistent with the County's overall Pavement Management Program.	\$4,246,000	\$0
Passenger Terminal Hold Room Expansion	The project involves an Airport Passenger Terminal Hold Room expansion to increase passenger capacity.	3,875,000	2,000
Smithsburg WWTP ENR Upgrade	The project will upgrade the facility to address Maryland Department of the Environment (MDE) strategy for Enhanced Nutrient Removal (ENR) and expand capacity to address growth needs of the area.	3,000,000	10,000
Urban Education Campus-BOE Component	This represents the Board's and State component of the intergovernmental partnership that supports the revitalization of the urban core of Hagerstown.	2,552,000	0
Eastern Boulevard Widening Phase II	The project serves to rehabilitate and widen Eastern Boulevard from Security Road to Antietam Drive from the existing 2-lane roadway to a 4-lane divided roadway, including drainage improvements and an at grade railroad crossing.	2,081,000	2,000
Halfway Boulevard Extended	The project involves the construction of a new four-lane open section roadway, including a large culvert, connecting existing Halfway Boulevard to MD Route 63. Intersection improvements at Halfway Boulevard extended and MD Route 63 include a traffic signal.	2,000,000	2,000
Police & EMS Training Facility	As the need for police, fire, and emergency services continues to grow, the need for a facility for training emergency personnel grows as well. Such a facility would be centralized to the area, easily accessible, and utilized by state, county and local police departments, correctional guards, and security guards. Classroom training for fire and emergency services departments, that predominantly takes place currently in local fire houses, would be held in this facility. With the potential to utilize previously owned County property, the facility would allow for local law enforcement and Fire/EMS programs to migrate from the HCC facilities and have a designated public safety campus. The project would be constructed over several phases ranging from the main building to other necessary training structures.	1,900,000	500,000



Project Name	Project Description	Project Budget	Operating Impact
Halfway Boulevard Bridges W0912	The project involves repairing the eastbound and westbound bridges of Halfway Boulevard over the Norfolk Southern railroad (ADC Map 20, H-8). The project will include cleaning and painting of steel beams, replacement of bearings and expansion joints, concrete repairs to the substructure and superstructure, traffic barrier improvements, and minor paving work.	\$1,132,000	\$0
Passenger Boarding Bridge	This project will procure and install a passenger boarding bridge.	1,124,000	500
ARCC Air Conditioning	The ARCC (Athletic Recreation and Community Center) is the largest indoor gathering spot in Washington County, with no other facility approaching its capacity. As the largest indoor gathering spot in Washington County, the ARCC facility is in need of a space where it is climate controlled. A variety of college and community activities are held in the ARCC. These include credit and credit-free courses, educational programs, athletic events and community events. Also housed within the facility is the Washington County Recreation Department (WCRD). Approximately 50 percent of the WCRD programming takes place in the ARCC. Along with the College and WCRD activities, the ARCC also hosts community and regional events, including high school graduations, athletic competitions, home shows and other events. To increase programming for the College, WCRD, and community events, the facility needs to install air conditioning in the arena to accommodate and increase the high level of usage.	1,100,000	75,000
Highway - Vehicle & Equipment Replacement Program	This project will replace vehicles and heavy/specialized equipment for maintenance and construction activity.	1,068,000	0
Frog Eye Road Culvert 11/06	This project is located in the 19800 block of Frog Eye Road (ADC Map 37, Grid D-4). The project will replace a single lane steel stringer/open steel grid deck with a two-cell box culvert with headwalls. The project will include realignment of the road, use of existing bridge for access during construction, and the installation of an appropriate traffic barrier.	652,000	0
Law Enforcement - Vehicle & Equipment Replacement Program	This project is for the replacement of public safety fleet inventory.	520,000	0
Elementary School 1	New 5-round facility to provide capacity to close 2 aging school facilities.	500,000	-1,249,000

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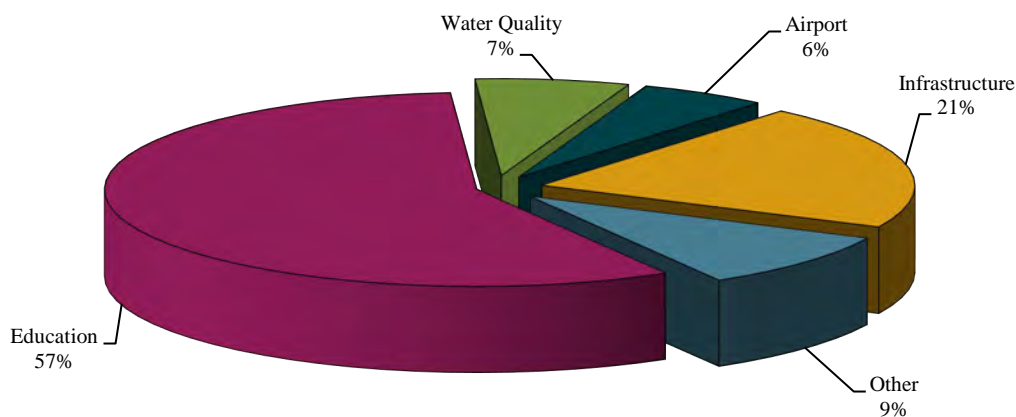
# Ten Year Summary

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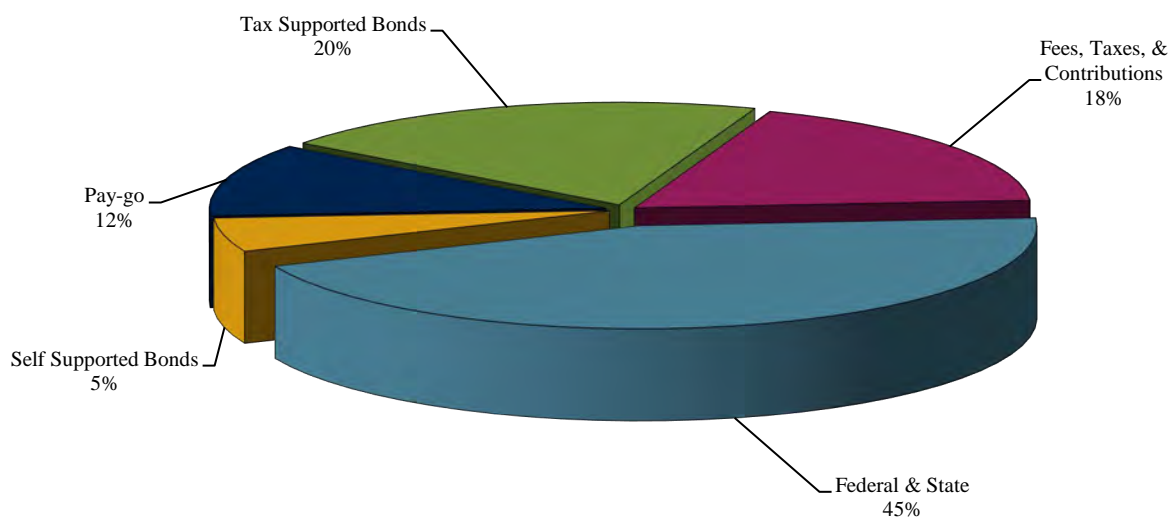
### *Ten Year Capital Improvement Plan Graphs*

The following graphs illustrate the projects by category and the funding sources that are currently scheduled for FY2021 thru FY2030. The 'Other' category includes projects for: Parks & Recreation, Transit, General Government, and Public Safety. The 'Education' category includes projects for the Board of Education, the Hagerstown Community College, and Public Libraries. The total for all projects represented in the chart is \$591,995,000.

Project Categories for FY2021 thru FY2030



Funding Sources for FY2021 thru FY2030



*Capital Improvement Ten Year Summary*  
*Fiscal Year 2021 – 2030*

Project	Total	Prior Appr.	FY 2021	FY 2022	FY 2023	FY 2024	FY 2025	FY 2026	Future
<b>Project Costs</b>									
Airport	40,089,595	7,179,595	5,161,000	2,295,000	6,064,000	6,394,000	7,371,000	1,305,000	4,320,000
Bridges	18,795,511	3,978,511	1,930,000	2,042,000	2,733,000	578,000	518,000	787,000	6,229,000
Drainage	16,790,881	3,983,881	661,000	1,075,000	975,000	1,550,000	1,682,000	1,274,000	5,590,000
Education	388,354,980	49,131,980	21,503,000	24,715,000	29,716,000	36,849,000	35,653,000	34,468,000	156,319,000
General Government	12,543,004	2,313,004	609,000	1,004,000	1,016,000	1,015,000	1,092,000	1,205,000	4,289,000
Parks & Recreation	8,774,708	594,708	574,000	820,000	690,000	960,000	860,000	815,000	3,461,000
Public Safety	39,932,568	9,239,568	2,603,000	3,570,000	3,482,000	2,528,000	2,761,000	3,581,000	12,168,000
Railroad Crossings	2,044,837	669,837	0	295,000	0	0	348,000	0	732,000
Road Improvement	118,423,859	23,334,859	10,531,000	8,450,000	8,642,000	9,922,000	9,519,000	9,198,000	38,827,000
Solid Waste	7,424,698	368,698	373,000	1,989,000	58,000	499,000	3,536,000	236,000	365,000
Transit	11,626,230	4,548,230	545,000	1,027,000	465,000	465,000	375,000	465,000	3,736,000
Water Quality	45,004,392	12,467,392	10,384,000	1,104,000	2,354,000	1,368,000	2,102,000	1,974,000	13,251,000
<b>TOTAL</b>	<b>709,805,263</b>	<b>117,810,263</b>	<b>54,874,000</b>	<b>48,386,000</b>	<b>56,195,000</b>	<b>62,128,000</b>	<b>65,817,000</b>	<b>55,308,000</b>	<b>249,287,000</b>
<b>Funding Sources</b>									
General Fund	70,616,322	15,016,322	800,000	5,700,000	5,850,000	5,750,000	6,000,000	6,000,000	25,500,000
Highway Fund	6,111,027	1,111,027	500,000	500,000	500,000	500,000	500,000	500,000	2,000,000
Cascade Fund	13,000	13,000	0	0	0	0	0	0	0
Solid Waste Fund	1,250,698	368,698	183,000	88,000	58,000	59,000	64,000	65,000	365,000
Utility Admin Fund	1,598,714	388,714	180,000	105,000	105,000	103,000	108,000	109,000	500,000
Water Fund	276,339	126,339	15,000	15,000	15,000	15,000	15,000	15,000	60,000
Sewer Fund	3,755,828	2,293,828	194,000	174,000	174,000	115,000	115,000	125,000	565,000
Airport Fund	1,103,220	248,220	57,000	73,000	98,000	94,000	115,000	125,000	293,000
Tax-Supported Bond	144,114,319	24,114,319	12,000,000	12,000,000	12,000,000	12,000,000	12,000,000	12,000,000	48,000,000
Self-Supported Bond	40,715,111	9,341,111	10,010,000	1,496,000	1,710,000	1,425,000	5,011,000	1,896,000	9,826,000
Transfer Tax	23,966,279	3,966,279	2,000,000	2,000,000	2,000,000	2,000,000	2,000,000	2,000,000	8,000,000
Excise Tax - Schools	5,005,000	1,155,000	385,000	385,000	385,000	385,000	385,000	385,000	1,540,000
Excise Tax - Roads	1,530,320	270,320	126,000	126,000	126,000	126,000	126,000	126,000	504,000
Excise Tax - Other	319,000	29,000	29,000	29,000	29,000	29,000	29,000	29,000	116,000
Excise Tax - Library	163,900	63,900	10,000	10,000	10,000	10,000	10,000	10,000	40,000
Excise Tax - Non-Residential	550,000	50,000	50,000	50,000	50,000	50,000	50,000	50,000	200,000
APFO Fees - Roads	300,000	300,000	0	0	0	0	0	0	0
Capital Reserve - General	4,493,000	1,956,000	2,537,000	0	0	0	0	0	0
Capital Reserve - Water	125,000	125,000	0	0	0	0	0	0	0
Federal Grant	58,862,057	15,707,057	8,071,000	4,886,000	7,021,000	6,192,000	7,070,000	1,684,000	8,231,000
State Grant	254,325,379	32,567,379	9,891,000	16,846,000	20,283,000	24,774,000	24,586,000	22,440,000	102,938,000
Contributions	90,610,750	8,598,750	7,836,000	3,903,000	5,781,000	8,501,000	7,633,000	7,749,000	40,609,000
<b>TOTAL</b>	<b>709,805,263</b>	<b>117,810,263</b>	<b>54,874,000</b>	<b>48,386,000</b>	<b>56,195,000</b>	<b>62,128,000</b>	<b>65,817,000</b>	<b>55,308,000</b>	<b>249,287,000</b>

*Project Detail of Major Projects – Fiscal Years 2021-2030*

Project Name	Project Description	Ten Year Project Budget
Capital Maintenance - BOE	Projects vary depending on the conditions, safety, security, and utility requirements. The Comprehensive Maintenance Plan outlines specific projects over the next five years. Projects which qualify for State funding of 79% of construction costs are included here. Projects are targeted to reduce deferred maintenance.	\$118,423,000
Pavement Maintenance and Rehab Program	This project includes the modification of county-wide pavement maintenance program targeting rehabilitation of county highway pavement, as required. Techniques may include but not be limited to road reclamation, bituminous concrete overlay, crack sealing, and surface treatment. Individual projects will be determined on an annual basis consistent with the County's overall Pavement Management Program.	51,996,000
Elementary School 1	New 5-round facility to provide capacity to close 2 aging school facilities.	43,806,000
Elementary School 2	New 5-round facility to provide capacity to allow closing of 3 aging school facilities.	43,806,000
Elementary School 3	New 5-round facility to provide capacity to allow closing of 2 aging school facilities.	43,806,000
Elementary School 4	New 5-round facility to provide capacity to allow closing of 2 aging school facilities.	42,000,000
Highway - Vehicle & Equipment Replacement Program	This project will replace vehicles and heavy/specialized equipment for maintenance and construction activity.	13,748,000
Land Acquisition-Airport	This project is for land acquisition to promote future economic development of the Airport. FY2022- Topflight corner for Runway 20 Viability Zone and 14223 Oak Springs. FY2023-Acquire Land for Development FY2025-Aquire Land for Development	13,334,000
Jonathan Hager Elementary School Addition	Provide additional 314 seat capacity to a maximum of 785 students.	12,219,000

*Capital Improvement Ten Year Detail*  
*Fiscal Year 2021- 2030*

Project	Total	Prior Appr.	Budget Year	Ten Year Capital Program					
			FY 2021	FY 2022	FY 2023	FY 2024	FY 2025	FY 2026	Future
Project Costs									
<u>Airport</u>									
Passenger Terminal Hold Room Expansion	7,612,000	3,737,000	3,875,000	0	0	0	0	0	0
T-Hangar 1, 2, & 3 Replacement	425,000	103,000	9,000	33,000	28,000	34,000	35,000	35,000	148,000
Airport Roof Replacement Project	374,220	99,220	15,000	15,000	45,000	35,000	55,000	25,000	85,000
Passenger Boarding Bridge	1,124,000	0	1,124,000	0	0	0	0	0	0
Capital Equipment - Airport	1,429,375	733,375	38,000	25,000	135,000	25,000	25,000	65,000	383,000
Land Acquisition-Airport	15,841,000	2,507,000	0	2,222,000	5,556,000	0	5,556,000	0	0
Wildlife Assessment	100,000	0	100,000	0	0	0	0	0	0
Proposed Taxiway S	1,180,000	0	0	0	0	0	0	1,180,000	0
Runway 9 MALSR	1,484,000	0	0	0	0	0	0	0	1,484,000
Snow Removal Equipment Storage Building Expansion	2,000,000	0	0	0	0	300,000	1,700,000	0	0
Taxiway F Rehabilitation	6,300,000	0	0	0	300,000	6,000,000	0	0	0
Taxiway G Rehabilitation	1,920,000	0	0	0	0	0	0	0	1,920,000
Taxiway H Rehabilitation	300,000	0	0	0	0	0	0	0	300,000
Airport Total	40,089,595	7,179,595	5,161,000	2,295,000	6,064,000	6,394,000	7,371,000	1,305,000	4,320,000
<u>Bridges</u>									
Bridge Inspection and Inventory	699,250	287,250	0	22,000	0	171,000	0	24,000	195,000
Roxbury Road Bridge W5372	3,892,077	2,892,077	0	500,000	500,000	0	0	0	0
Bridge Scour Repairs	513,184	31,184	0	0	0	0	0	232,000	250,000
Cleaning & Painting of Steel Bridges	408,000	108,000	0	150,000	150,000	0	0	0	0
Halfway Boulevard Bridges W0912	4,489,000	575,000	1,132,000	1,182,000	1,600,000	0	0	0	0
Keefer Road Bridge 15/20	231,000	85,000	146,000	0	0	0	0	0	0
Frog Eye Road Culvert 11/06	652,000	0	652,000	0	0	0	0	0	0
Appletown Road Bridge W2184	474,000	0	0	0	0	0	0	0	474,000
Ashton Road Culvert 04/06	30,000	0	0	0	0	0	0	0	30,000
Bowie Road Culvert	305,000	0	0	0	0	0	0	0	305,000
Burnside Bridge Road Culvert 01/03	329,000	0	0	0	0	0	114,000	215,000	0
Draper Road Culvert 04/07	428,000	0	0	0	0	0	0	0	428,000
Greenspring Furnace Road Culvert 15/15	398,000	0	0	188,000	210,000	0	0	0	0
Gruber Road Bridge 04/10	10,000	0	0	0	0	0	0	0	10,000
Harpers Ferry Road Culvert 11/02	541,000	0	0	0	0	0	0	82,000	459,000
Henline Road Culvert 05/05	465,000	0	0	0	0	0	0	0	465,000

Project	Total	Prior Appr.	Budget Year	Ten Year Capital Program					
			FY 2021	FY 2022	FY 2023	FY 2024	FY 2025	FY 2026	Future
Hoffman's Inn Road Culvert 05/06	313,000	0	0	0	0	0	0	0	313,000
Kretsinger Road Culvert 14/01	316,000	0	0	0	137,000	179,000	0	0	0
Lanes Road Culvert 15/12	317,000	0	0	0	0	32,000	285,000	0	0
Long Hollow Road Culvert 05/07	316,000	0	0	0	0	0	0	66,000	250,000
Mooreville Road Culvert 15/21	355,000	0	0	0	0	0	0	0	355,000
Remsburg Road Culvert	287,000	0	0	0	0	0	119,000	168,000	0
Rinehart Road Culvert 14/03	332,000	0	0	0	136,000	196,000	0	0	0
Stone Masonry Bridge Repairs	270,000	0	0	0	0	0	0	0	270,000
Willow Road Culvert 05/10	323,000	0	0	0	0	0	0	0	323,000
Yarrowsburg Road Bridge W6191	2,102,000	0	0	0	0	0	0	0	2,102,000
<b>Bridges Total</b>	<b>18,795,511</b>	<b>3,978,511</b>	<b>1,930,000</b>	<b>2,042,000</b>	<b>2,733,000</b>	<b>578,000</b>	<b>518,000</b>	<b>787,000</b>	<b>6,229,000</b>
<b><u>Drainage</u></b>									
Stream Restoration at Various Locations	2,080,782	594,782	250,000	100,000	0	381,000	0	0	755,000
Stormwater Retrofits	12,413,099	3,314,099	336,000	900,000	900,000	962,000	979,000	996,000	4,026,000
Drainage Improvements at Various Locations	825,000	75,000	75,000	75,000	75,000	75,000	75,000	75,000	300,000
Broadfording Church Road Culvert	231,000	0	0	0	0	57,000	174,000	0	0
Draper Road Drainage Improvements	509,000	0	0	0	0	0	0	0	509,000
Harpers Ferry Road Drainage, 3600 Block	376,000	0	0	0	0	75,000	301,000	0	0
Shank Road Drainage	153,000	0	0	0	0	0	153,000	0	0
University Road Culvert	203,000	0	0	0	0	0	0	203,000	0
<b>Drainage Total</b>	<b>16,790,881</b>	<b>3,983,881</b>	<b>661,000</b>	<b>1,075,000</b>	<b>975,000</b>	<b>1,550,000</b>	<b>1,682,000</b>	<b>1,274,000</b>	<b>5,590,000</b>
<b><u>Board of Education</u></b>									
Capital Maintenance - BOE	122,315,488	3,892,488	6,701,000	17,776,000	8,400,000	13,580,000	11,676,000	11,368,000	48,922,000
Sharpsburg Elementary School Replacement	26,728,000	20,537,000	6,191,000	0	0	0	0	0	0
Elementary School 1	43,806,000	0	500,000	2,000,000	19,806,000	19,500,000	2,000,000	0	0
Elementary School 2	43,806,000	0	0	0	500,000	2,000,000	19,768,000	19,400,000	2,138,000
Elementary School 3	43,806,000	0	0	0	0	0	500,000	2,000,000	41,306,000
Elementary School 4	42,000,000	0	0	0	0	0	0	0	42,000,000
Jonathan Hager Elementary School Addition	12,219,000	0	0	0	0	0	0	0	12,219,000
Urban Education Campus- BOE Component	19,400,000	16,848,000	2,552,000	0	0	0	0	0	0
<b>Board of Education Total</b>	<b>354,080,488</b>	<b>41,277,488</b>	<b>15,944,000</b>	<b>19,776,000</b>	<b>28,706,000</b>	<b>35,080,000</b>	<b>33,944,000</b>	<b>32,768,000</b>	<b>146,585,000</b>



Project	Total	Prior Appr.	Budget Year	Ten Year Capital Program					
			FY 2021	FY 2022	FY 2023	FY 2024	FY 2025	FY 2026	Future
<b><u>Hagerstown Community College</u></b>									
Center for Business and Entrepreneurial Studies	11,230,000	6,281,000	4,449,000	500,000	0	0	0	0	0
ARCC Air Conditioning	1,100,000	0	1,100,000	0	0	0	0	0	0
ATC Operations Building	5,400,000	0	0	0	0	0	958,000	1,690,000	2,752,000
Campus Road & Parking Lot Overlays	3,780,000	0	0	0	400,000	0	0	0	3,380,000
Career Programs Roof Replacement	1,562,000	0	0	0	0	0	0	0	1,562,000
CVT Instructional Facility Acquisition	2,000,000	1,400,000	0	0	600,000	0	0	0	0
CVT Instructional Facility Renovation/ Construction Project	2,500,000	0	0	0	0	1,759,000	741,000	0	0
LRC Exterior Metal Panel System and Roof Replacement	1,746,000	0	0	1,746,000	0	0	0	0	0
Second Entrance Drive Widening Project	2,000,000	0	0	0	0	0	0	0	2,000,000
<b>Hagerstown Community College Total</b>	<b>31,318,000</b>	<b>7,681,000</b>	<b>5,549,000</b>	<b>2,246,000</b>	<b>1,000,000</b>	<b>1,759,000</b>	<b>1,699,000</b>	<b>1,690,000</b>	<b>9,694,000</b>
<b><u>Public Libraries</u></b>									
Systemic Projects - Library	131,492	41,492	0	10,000	10,000	10,000	10,000	10,000	40,000
Hancock Public Library Replacement	2,825,000	132,000	10,000	2,683,000	0	0	0	0	0
<b>Public Libraries Total</b>	<b>2,956,492</b>	<b>173,492</b>	<b>10,000</b>	<b>2,693,000</b>	<b>10,000</b>	<b>10,000</b>	<b>10,000</b>	<b>10,000</b>	<b>40,000</b>
<b><u>General Government</u></b>									
Cost of Bond Issuance	1,203,875	143,875	98,000	98,000	99,000	98,000	99,000	98,000	470,000
Contingency - General Fund	1,846,933	1,021,933	0	75,000	75,000	75,000	100,000	100,000	400,000
Systemic Improvements - Building	2,584,589	654,589	150,000	197,000	196,000	196,000	196,000	197,000	798,000
Facilities Roof Repairs	1,330,000	100,000	0	130,000	100,000	100,000	150,000	275,000	475,000
Information Systems Replacement Program	1,146,460	146,460	100,000	100,000	100,000	100,000	100,000	100,000	400,000
Financial System Management & Upgrades	427,590	110,590	0	33,000	34,000	34,000	35,000	35,000	146,000
County Wireless Infrastructure	73,745	14,745	11,000	12,000	12,000	12,000	12,000	0	0
General - Equipment and Vehicle Replacement Program	3,929,812	120,812	250,000	359,000	400,000	400,000	400,000	400,000	1,600,000
<b>General Government Total</b>	<b>12,543,004</b>	<b>2,313,004</b>	<b>609,000</b>	<b>1,004,000</b>	<b>1,016,000</b>	<b>1,015,000</b>	<b>1,092,000</b>	<b>1,205,000</b>	<b>4,289,000</b>
<b><u>Parks &amp; Recreation</u></b>									
BR Capital Equipment Program	368,618	4,618	64,000	25,000	25,000	30,000	30,000	35,000	155,000
BR Golf Simulator	20,000	0	20,000	0	0	0	0	0	0

Project	Total	Prior Appr.	Budget Year	Ten Year Capital Program					
			FY 2021	FY 2022	FY 2023	FY 2024	FY 2025	FY 2026	Future
Black Rock Club House Renovation	75,000	0	0	75,000	0	0	0	0	0
Tennis Court Resurfacing	312,990	42,990	0	30,000	30,000	30,000	30,000	30,000	120,000
Park Equipment/Surfacing Replacement, Various Locations	1,917,000	242,000	150,000	150,000	150,000	175,000	175,000	175,000	700,000
Parking Lot Repair/Overlay, Various Locations	805,100	305,100	50,000	50,000	50,000	50,000	50,000	50,000	200,000
Antietam Creek Water Trail	65,000	0	25,000	40,000	0	0	0	0	0
County Parks - Bathroom/Drinking Fountain Upgrades	250,000	0	50,000	50,000	50,000	50,000	50,000	0	0
Doubs Woods Equipment Storage Building	150,000	0	150,000	0	0	0	0	0	0
MLK Gymnasium Upgrade	200,000	0	25,000	175,000	0	0	0	0	0
Woodland Way Sidewalks and Pathways	40,000	0	40,000	0	0	0	0	0	0
Ag Center Show Area Floor	50,000	0	0	0	0	50,000	0	0	0
Conococheague Creek Water Trail	210,000	0	0	0	10,000	200,000	0	0	0
Marty Snook Dog Park	25,000	0	0	25,000	0	0	0	0	0
Marty Snook Park Field Improvements	50,000	0	0	0	0	50,000	0	0	0
Marty Snook Park Fitness Trail	300,000	0	0	0	150,000	150,000	0	0	0
Marty Snook Park Pool Accessible Entrance	100,000	0	0	0	100,000	0	0	0	0
North Central County Park	3,286,000	0	0	0	100,000	0	500,000	500,000	2,186,000
Recreational Field Bleachers	75,000	0	0	0	0	75,000	0	0	0
Regional Park Dog Park	75,000	0	0	0	0	75,000	0	0	0
Regional Park Walking/Hiking Trail	200,000	0	0	200,000	0	0	0	0	0
Roof Replacements Various Locations	200,000	0	0	0	25,000	25,000	25,000	25,000	100,000
<b>Parks &amp; Recreation Total</b>	<b>8,774,708</b>	<b>594,708</b>	<b>574,000</b>	<b>820,000</b>	<b>690,000</b>	<b>960,000</b>	<b>860,000</b>	<b>815,000</b>	<b>3,461,000</b>
<b><u>Public Safety</u></b>									
Detention Center - Systemic Projects	3,029,323	582,323	0	450,000	450,000	300,000	270,000	20,000	957,000
Police & EMS Training Facility	12,000,000	7,300,000	1,900,000	1,400,000	1,400,000	0	0	0	0
Communication Tower(s) Various	642,526	178,526	0	110,000	0	114,000	0	118,000	122,000
Motorola Portable Radio Replacement Program	1,268,821	106,821	108,000	110,000	112,000	114,000	116,000	118,000	484,000
Law Enforcement - Vehicle & Equipment Replacement Program	7,354,715	449,715	520,000	650,000	660,000	650,000	725,000	725,000	2,975,000
Emergency Services Equipment & Vehicle Program	2,357,183	622,183	75,000	150,000	160,000	150,000	200,000	200,000	800,000

Project	Total	Prior Appr.	Budget Year	Ten Year Capital Program					
			FY 2021	FY 2022	FY 2023	FY 2024	FY 2025	FY 2026	Future
911 Center Building Expansion	1,800,000	0	0	0	0	0	250,000	1,200,000	350,000
Class A Burn Building - HFD Training Center	500,000	0	0	500,000	0	0	0	0	0
P25 UHF Public Safety Radio Communications System Upgrade	5,800,000	0	0	200,000	700,000	700,000	700,000	700,000	2,800,000
Patrol Services Relocation Renovation	5,180,000	0	0	0	0	500,000	500,000	500,000	3,680,000
<b>Public Safety Total</b>	<b>39,932,568</b>	<b>9,239,568</b>	<b>2,603,000</b>	<b>3,570,000</b>	<b>3,482,000</b>	<b>2,528,000</b>	<b>2,761,000</b>	<b>3,581,000</b>	<b>12,168,000</b>
<b><u>Railroad Crossings</u></b>									
Railroad Crossing Improvements	2,044,837	669,837	0	295,000	0	0	348,000	0	732,000
<b>Railroad Crossings Total</b>	<b>2,044,837</b>	<b>669,837</b>	<b>0</b>	<b>295,000</b>	<b>0</b>	<b>0</b>	<b>348,000</b>	<b>0</b>	<b>732,000</b>
<b><u>Road Improvement</u></b>									
Transportation ADA	1,195,450	351,450	82,000	83,000	84,000	86,000	87,000	84,000	338,000
Pavement Maintenance and Rehab Program	60,818,787	8,822,787	4,246,000	5,000,000	5,250,000	5,250,000	5,250,000	5,250,000	21,750,000
Longmeadow Road	810,000	0	0	0	0	0	310,000	500,000	0
Eastern Boulevard Extended	7,850,000	0	0	0	0	1,347,000	804,000	1,556,000	4,143,000
Eastern Boulevard Widening Phase II	5,672,300	2,691,300	2,081,000	500,000	400,000	0	0	0	0
Eastern Blvd at Antietam Drive Improvements	2,506,000	2,006,000	0	0	500,000	0	0	0	0
Professional Boulevard Extended Phase II	4,309,200	4,059,200	250,000	0	0	0	0	0	0
Valley Mall Area Road Improvements Phase II	942,000	200,000	0	0	0	142,000	600,000	0	0
Professional Boulevard Extended Phase III	1,203,000	0	0	0	121,000	1,082,000	0	0	0
Showalter Road Extended East	2,983,000	510,000	0	0	0	0	0	0	2,473,000
Halfway Boulevard Extended	5,973,000	3,073,000	2,000,000	400,000	500,000	0	0	0	0
Wright Road	2,750,000	0	454,000	1,268,000	528,000	500,000	0	0	0
Burnside Bridge Road Spot Improvements	544,000	0	0	0	0	0	0	0	544,000
Colonel Henry Douglas Drive Extended Phase II	400,000	0	0	0	0	0	0	0	400,000
E. Oak Ridge Drive/South Pointe Signal	461,000	0	0	0	0	0	111,000	350,000	0
Mt Aetna Road Spot Improvements	1,727,000	0	0	0	0	0	0	0	1,727,000
Professional Boulevard Extended - Phase IV	800,000	0	0	0	0	0	800,000	0	0
Rockdale Road and Independence Road Spot Improvements	1,025,000	0	0	0	0	0	0	0	1,025,000
Highway - Vehicle & Equipment Replacement Program	14,883,122	1,135,122	1,068,000	1,000,000	1,084,000	1,322,000	1,389,000	1,458,000	6,427,000

Project	Total	Prior Appr.	Budget Year	Ten Year Capital Program					
			FY 2021	FY 2022	FY 2023	FY 2024	FY 2025	FY 2026	Future
Highway Maintenance Shop - Western Section	374,000	0	0	199,000	175,000	0	0	0	0
Salt Storage Structure	350,000	0	350,000	0	0	0	0	0	0
Highway Western Section - Fuel Tank Replacement	847,000	486,000	0	0	0	193,000	168,000	0	0
<b>Road Improvement Total</b>	<b>118,423,859</b>	<b>23,334,859</b>	<b>10,531,000</b>	<b>8,450,000</b>	<b>8,642,000</b>	<b>9,922,000</b>	<b>9,519,000</b>	<b>9,198,000</b>	<b>38,827,000</b>
<b><u>Solid Waste</u></b>									
Contingency - Solid Waste	458,000	83,000	0	0	30,000	30,000	35,000	35,000	245,000
Close Out Cap - Rubblefill	2,091,000	0	190,000	1,901,000	0	0	0	0	0
SW Equipment & Vehicle Replacement	576,698	285,698	27,000	28,000	28,000	29,000	29,000	30,000	120,000
City/County Groundwater Investigation	156,000	0	156,000	0	0	0	0	0	0
40 West Landfill - Cell 5 Construction	4,083,000	0	0	0	0	440,000	3,472,000	171,000	0
40 West Security Upgrades	60,000	0	0	60,000	0	0	0	0	0
<b>Solid Waste Total</b>	<b>7,424,698</b>	<b>368,698</b>	<b>373,000</b>	<b>1,989,000</b>	<b>58,000</b>	<b>499,000</b>	<b>3,536,000</b>	<b>236,000</b>	<b>365,000</b>
<b><u>Transit</u></b>									
Vehicle Preventive Maintenance	4,234,230	484,230	375,000	375,000	375,000	375,000	375,000	375,000	1,500,000
Fixed Route Bus Replacement Program	6,580,000	3,972,000	0	652,000	0	0	0	0	1,956,000
ADA Bus Replacement	540,000	0	90,000	0	90,000	90,000	0	90,000	180,000
Support Vehicles	80,000	0	80,000	0	0	0	0	0	0
Transportation Development Plan	192,000	92,000	0	0	0	0	0	0	100,000
<b>Transit Total</b>	<b>11,626,230</b>	<b>4,548,230</b>	<b>545,000</b>	<b>1,027,000</b>	<b>465,000</b>	<b>465,000</b>	<b>375,000</b>	<b>465,000</b>	<b>3,736,000</b>
<b><u>Water Quality</u></b>									
<b><u>Utility Administration</u></b>									
Contingency - Utility Admin	166,677	135,677	0	0	0	0	0	0	31,000
General Building Improvements	964,000	0	0	0	0	370,000	594,000	0	0
Lab Equipment Replacement	354,117	30,117	105,000	25,000	25,000	23,000	23,000	24,000	99,000
WQ Equip/Vehicle Replacement Program	1,107,320	252,320	75,000	80,000	80,000	80,000	85,000	85,000	370,000
<b>Utility Administration Total</b>	<b>2,592,114</b>	<b>418,114</b>	<b>180,000</b>	<b>105,000</b>	<b>105,000</b>	<b>473,000</b>	<b>702,000</b>	<b>109,000</b>	<b>500,000</b>
<b><u>Wastewater Utility</u></b>									
Contingency - Sewer	86,939	36,939	0	0	0	0	0	0	50,000
Replace Grinder Pumps	890,418	50,418	60,000	80,000	80,000	80,000	80,000	90,000	370,000
Pen Mar/ Highfield/ Cascade Septic Tank Pumping and Replacement	115,000	75,000	40,000	0	0	0	0	0	0
Pump Station Upgrades - Various Stations	4,340,803	1,527,803	275,000	0	153,000	0	0	885,000	1,500,000
Collection System Rehabilitation Project	3,452,618	372,618	0	560,000	0	0	870,000	0	1,650,000

Project	Total	Prior Appr.	Budget Year	Ten Year Capital Program					
			FY 2021	FY 2022	FY 2023	FY 2024	FY 2025	FY 2026	Future
Capacity Management Project	10,900,542	4,460,542	6,440,000	0	0	0	0	0	0
Smithsburg WWTP ENR Upgrade	12,903,387	4,403,387	3,000,000	0	0	0	0	0	5,500,000
Heavy Sewer EQP and VEH Replacement	893,934	361,934	94,000	94,000	94,000	35,000	35,000	35,000	145,000
Potomac Edison Pump Station & Force Main	1,700,000	0	0	0	0	0	0	0	1,700,000
General WwTP Improvements	1,350,000	0	0	0	250,000	250,000	300,000	250,000	300,000
<b>Wastewater Utility Total</b>	<b>36,633,641</b>	<b>11,288,641</b>	<b>9,909,000</b>	<b>734,000</b>	<b>577,000</b>	<b>365,000</b>	<b>1,285,000</b>	<b>1,260,000</b>	<b>11,215,000</b>
<b>Water Utility</b>									
Sharpsburg Water Meter Cradle Replacement	1,000,000	250,000	125,000	250,000	250,000	125,000	0	0	0
Water Meter Replacement	260,339	110,339	15,000	15,000	15,000	15,000	15,000	15,000	60,000
Mt Aetna Water System Improvements	729,000	130,000	0	0	599,000	0	0	0	0
Cascade Town Center Water System Improvements	125,000	0	125,000	0	0	0	0	0	0
Sharpsburg Water Treatment Plant	794,000	204,000	0	0	0	0	0	590,000	0
General WTP Improvements	1,168,298	66,298	30,000	0	242,000	290,000	0	0	540,000
Highfield/Sharpsburg Water Storage Tank	336,000	0	0	0	0	0	0	0	336,000
WQ Main Replacement	1,366,000	0	0	0	566,000	100,000	100,000	0	600,000
<b>Water Utility Total</b>	<b>5,778,637</b>	<b>760,637</b>	<b>295,000</b>	<b>265,000</b>	<b>1,672,000</b>	<b>530,000</b>	<b>115,000</b>	<b>605,000</b>	<b>1,536,000</b>
<b>Water Quality Total</b>	<b>45,004,392</b>	<b>12,467,392</b>	<b>10,384,000</b>	<b>1,104,000</b>	<b>2,354,000</b>	<b>1,368,000</b>	<b>2,102,000</b>	<b>1,974,000</b>	<b>13,251,000</b>
<b>TOTAL</b>	<b>709,805,263</b>	<b>117,810,263</b>	<b>54,874,000</b>	<b>48,386,000</b>	<b>56,195,000</b>	<b>62,128,000</b>	<b>65,817,000</b>	<b>55,308,000</b>	<b>249,287,000</b>

Project	Total	Prior Appr.	Budget Year		Ten Year Capital Program					Future
			FY 2021	FY 2022	FY 2023	FY 2024	FY 2025	FY 2026		
Funding Sources										
General Fund	70,616,322	15,016,322	800,000	5,700,000	5,850,000	5,750,000	6,000,000	6,000,000	25,500,000	
Highway Fund	6,111,027	1,111,027	500,000	500,000	500,000	500,000	500,000	500,000	2,000,000	
Cascade Fund	13,000	13,000	0	0	0	0	0	0	0	
Solid Waste Fund	1,250,698	368,698	183,000	88,000	58,000	59,000	64,000	65,000	365,000	
Utility Admin Fund	1,598,714	388,714	180,000	105,000	105,000	103,000	108,000	109,000	500,000	
Water Fund	276,339	126,339	15,000	15,000	15,000	15,000	15,000	15,000	60,000	
Sewer Fund	3,755,828	2,293,828	194,000	174,000	174,000	115,000	115,000	125,000	565,000	
Airport Fund	1,103,220	248,220	57,000	73,000	98,000	94,000	115,000	125,000	293,000	
Tax-Supported Bond	144,114,319	24,114,319	12,000,000	12,000,000	12,000,000	12,000,000	12,000,000	12,000,000	48,000,000	
Self-Supported Bond	40,715,111	9,341,111	10,010,000	1,496,000	1,710,000	1,425,000	5,011,000	1,896,000	9,826,000	
Transfer Tax	23,966,279	3,966,279	2,000,000	2,000,000	2,000,000	2,000,000	2,000,000	2,000,000	8,000,000	
Excise Tax - Schools	5,005,000	1,155,000	385,000	385,000	385,000	385,000	385,000	385,000	1,540,000	
Excise Tax - Roads	1,530,320	270,320	126,000	126,000	126,000	126,000	126,000	126,000	504,000	
Excise Tax - Other	319,000	29,000	29,000	29,000	29,000	29,000	29,000	29,000	116,000	
Excise Tax - Library	163,900	63,900	10,000	10,000	10,000	10,000	10,000	10,000	40,000	
Excise Tax - Non-Residential	550,000	50,000	50,000	50,000	50,000	50,000	50,000	50,000	200,000	
APFO Fees - Roads	300,000	300,000	0	0	0	0	0	0	0	
Capital Reserve - General	4,493,000	1,956,000	2,537,000	0	0	0	0	0	0	
Capital Reserve - Water	125,000	125,000	0	0	0	0	0	0	0	
Federal Grant	58,862,057	15,707,057	8,071,000	4,886,000	7,021,000	6,192,000	7,070,000	1,684,000	8,231,000	
State Grant	254,325,379	32,567,379	9,891,000	16,846,000	20,283,000	24,774,000	24,586,000	22,440,000	102,938,000	
Contributions	90,610,750	8,598,750	7,836,000	3,903,000	5,781,000	8,501,000	7,633,000	7,749,000	40,609,000	
TOTAL	709,805,263	117,810,263	54,874,000	48,386,000	56,195,000	62,128,000	65,817,000	55,308,000	249,287,000	

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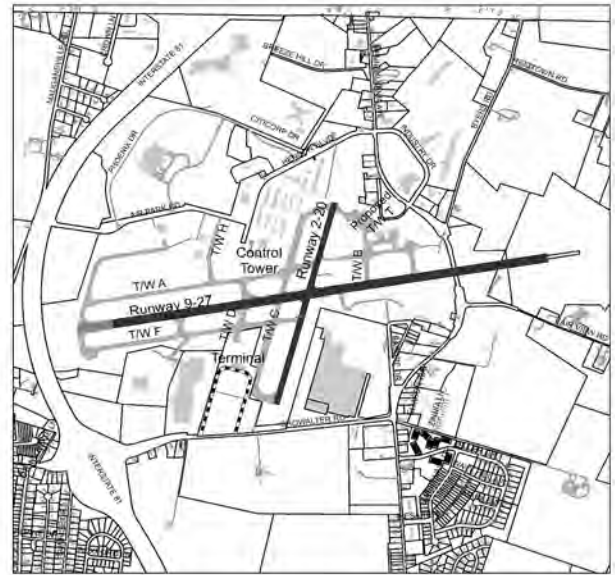
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*Airport*  
*Capital Improvement Ten Year Summary*  
*Fiscal Year 2021 - 2030*

Page	Project	Total	Prior Appr.	Budget Year		Ten Year Capital Program					Future
				FY 2021	FY 2022	FY 2023	FY 2024	FY 2025	FY 2026		
Project Costs											
Airport											
46	Passenger Terminal Hold Room Expansion	7,612,000	3,737,000	3,875,000	0	0	0	0	0	0	
47	T-Hangar 1, 2, & 3 Replacement	425,000	103,000	9,000	33,000	28,000	34,000	35,000	35,000	148,000	
48	Airport Roof Replacement Project	374,220	99,220	15,000	15,000	45,000	35,000	55,000	25,000	85,000	
49	Passenger Boarding Bridge	1,124,000	0	1,124,000	0	0	0	0	0	0	
50	Capital Equipment - Airport	1,429,375	733,375	38,000	25,000	135,000	25,000	25,000	65,000	383,000	
51	Land Acquisition-Airport	15,841,000	2,507,000	0	2,222,000	5,556,000	0	5,556,000	0	0	
52	Wildlife Assessment	100,000	0	100,000	0	0	0	0	0	0	
53	Proposed Taxiway S	1,180,000	0	0	0	0	0	0	1,180,000	0	
54	Runway 9 MALSR	1,484,000	0	0	0	0	0	0	0	1,484,000	
55	Snow Removal Equipment Storage Building Expansion	2,000,000	0	0	0	0	300,000	1,700,000	0	0	
56	Taxiway F Rehabilitation	6,300,000	0	0	0	300,000	6,000,000	0	0	0	
57	Taxiway G Rehabilitation	1,920,000	0	0	0	0	0	0	0	1,920,000	
58	Taxiway H Rehabilitation	300,000	0	0	0	0	0	0	0	300,000	
Airport Total		40,089,595	7,179,595	5,161,000	2,295,000	6,064,000	6,394,000	7,371,000	1,305,000	4,320,000	
Funding Sources											
General Fund		1,671,016	586,016	0	111,000	299,000	315,000	85,000	59,000	216,000	
Airport Fund		1,103,220	248,220	57,000	73,000	98,000	94,000	115,000	125,000	293,000	
Tax-Supported Bond		278,000	0	0	0	0	0	278,000	0	0	
Transfer Tax		111,000	0	111,000	0	0	0	0	0	0	
Capital Reserve - General		128,000	100,000	28,000	0	0	0	0	0	0	
Federal Grant		34,901,993	5,946,993	4,714,000	2,000,000	5,369,000	5,670,000	6,530,000	1,062,000	3,610,000	
State Grant		1,896,366	298,366	251,000	111,000	298,000	315,000	363,000	59,000	201,000	
		40,089,595	7,179,595	5,161,000	2,295,000	6,064,000	6,394,000	7,371,000	1,305,000	4,320,000	

<b>Project Title</b>	<b>Passenger Terminal Hold Room Expansion</b>
<b>Project ID</b>	BLD082
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$2,000



**Project Description** The project involves an Airport Passenger Terminal Hold Room expansion to increase passenger capacity.

**Project Assumptions** Grant funding approved:  
90% FAA share  
5% State share  
5% Local share

A FAA discretionary grant has been approved.

**Project Justification** The current security hold room is configured to accommodate up to 150 passengers. We currently host service from Allegiant using A320 aircraft configured to accommodate 186 passengers. It is our desire to add on to the existing hold room and increase the capacity to plan for current and future needs.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
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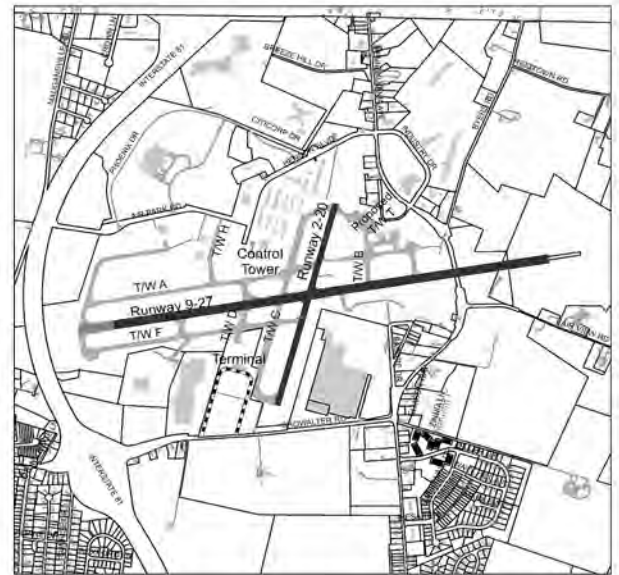
**Project Costs:**

Construction	6,791,000	3,535,000	3,256,000	0	0	0	0	0	0
Engineering and Design	821,000	202,000	619,000	0	0	0	0	0	0
<b>Total Cost</b>	<b>7,612,000</b>	<b>3,737,000</b>	<b>3,875,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

**Funding Sources:**

General Fund	261,000	261,000	0	0	0	0	0	0	0
Airport Fund	70,000	46,000	24,000	0	0	0	0	0	0
Transfer Tax	111,000	0	111,000	0	0	0	0	0	0
Capital Reserve - General	123,000	100,000	23,000	0	0	0	0	0	0
Federal Grant	6,676,000	3,176,000	3,500,000	0	0	0	0	0	0
State Grant	371,000	154,000	217,000	0	0	0	0	0	0
<b>Total Funding</b>	<b>7,612,000</b>	<b>3,737,000</b>	<b>3,875,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

<b>Project Title</b>	<b>T-Hangar 1, 2 and 3 Replacement</b>
<b>Project ID</b>	BLD087
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$0



<b>Project Description</b>	This project includes the demolition of existing T-hangars and the construction of new replacement T-hangars 1, 2 and 3. The total building size is 38,400 SF.
<b>Project Assumptions</b>	The project assumes a pre-engineered metal building. It is not anticipated that Federal Aviation Administration funding will be available for this project. Local and airport funds will be contributed each year to build up funds until the total requirement is achieved.
<b>Project Justification</b>	Existing buildings were built between 1963 and 1965 and have exceeded their useful life expectancy. Buildings have stone floors, and wood structural members. Doors are nonelectric manual open which is problematic and creates a continual maintenance problem.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Construction	375,000	103,000	9,000	0	11,000	34,000	35,000	35,000	148,000
Engineering and Design	50,000	0	0	33,000	17,000	0	0	0	0
<b>Total Cost</b>	<b>425,000</b>	<b>103,000</b>	<b>9,000</b>	<b>33,000</b>	<b>28,000</b>	<b>34,000</b>	<b>35,000</b>	<b>35,000</b>	<b>148,000</b>
<b><u>Funding Sources:</u></b>									
Airport Fund	425,000	103,000	9,000	33,000	28,000	34,000	35,000	35,000	148,000
<b>Total Funding</b>	<b>425,000</b>	<b>103,000</b>	<b>9,000</b>	<b>33,000</b>	<b>28,000</b>	<b>34,000</b>	<b>35,000</b>	<b>35,000</b>	<b>148,000</b>

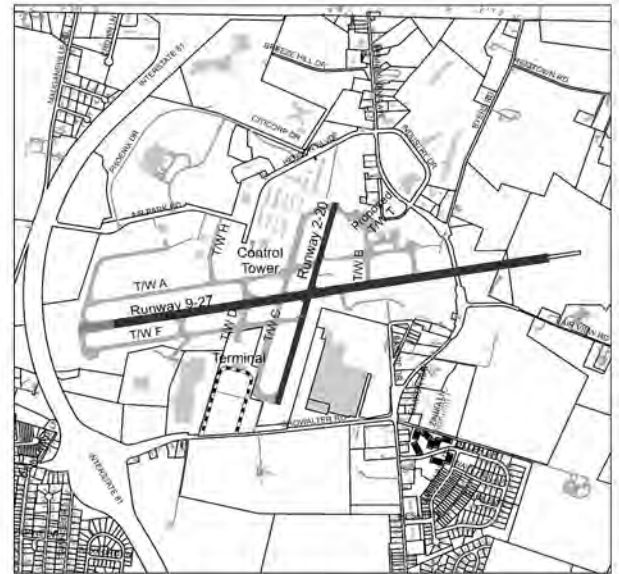
<b>Project Title</b>	<b>Airport Roof Replacement Project</b>
<b>Project ID</b>	BLD088
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$0



<b>Project Description</b>	Project will replace or refurbish various roof systems at the airport.
<b>Project Assumptions</b>	FY2021- 14221 Oak Springs Rd (shingle replacement)
<b>Project Justification</b>	An ongoing roof replacement/rehabilitation plan is necessary for all airport facilities. Airport will contribute funds on an annual basis to provide for this replacement plan.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	374,220	99,220	15,000	15,000	45,000	35,000	55,000	25,000	85,000
<b>Total Cost</b>	<b>374,220</b>	<b>99,220</b>	<b>15,000</b>	<b>15,000</b>	<b>45,000</b>	<b>35,000</b>	<b>55,000</b>	<b>25,000</b>	<b>85,000</b>
<b><u>Funding Sources:</u></b>									
Airport Fund	374,220	99,220	15,000	15,000	45,000	35,000	55,000	25,000	85,000
<b>Total Funding</b>	<b>374,220</b>	<b>99,220</b>	<b>15,000</b>	<b>15,000</b>	<b>45,000</b>	<b>35,000</b>	<b>55,000</b>	<b>25,000</b>	<b>85,000</b>

<b>Project Title</b>	<b>Passenger Boarding Bridge</b>
<b>Project ID</b>	BLD103
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$500



<b>Project Description</b>	This project will procure and install a passenger boarding bridge.
<b>Project Assumptions</b>	Pending grant funding approval: 90% FAA share 5% MAA share 5% Local share
<b>Project Justification</b>	The addition of a boarding bridge will increase passenger safety and experience.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	1,124,000	0	1,124,000	0	0	0	0	0	0
<b>Total Cost</b>	<b>1,124,000</b>	<b>0</b>	<b>1,124,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
Federal Grant	1,124,000	0	1,124,000	0	0	0	0	0	0
<b>Total Funding</b>	<b>1,124,000</b>	<b>0</b>	<b>1,124,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

<b>Project Title</b>	<b>Capital Equipment - Airport</b>
<b>Project ID</b>	EQP031
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$0

**Project Description** The planned purchases for FY21: Aircraft Foam Tester, Live Scan- digital fingerprint scanning. Future acquisitions are based on the Airport Equipment Replacement Program.

**Project Assumptions** Future equipment will be acquired pending FAA grant funding as follows:  
 90% FAA share  
 5% State share  
 5% Local share

Maryland Aviation Administration (MAA) Special Project grant funding: 75%  
 MAA share  
 25% Local share

**Project Justification** The Airport maintains an inventory of specialized heavy equipment and vehicles, including lawn maintenance equipment. Each year, the equipment is identified if replacement is necessary based on age and life expectancy. The goal of the replacement program is to balance replacement costs versus maintenance costs. In line with objectives of a well-managed organization, the Airport strives to establish an equipment replacement program that will set the equipment replacement cycle in line with best practices, minimizing maintenance costs.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Equipment/Furniture	1,429,375	733,375	38,000	25,000	135,000	25,000	25,000	65,000	383,000
<b>Total Cost</b>	<b>1,429,375</b>	<b>733,375</b>	<b>38,000</b>	<b>25,000</b>	<b>135,000</b>	<b>25,000</b>	<b>25,000</b>	<b>65,000</b>	<b>383,000</b>
<b><u>Funding Sources:</u></b>									
General Fund	237,016	200,016	0	0	6,000	0	0	0	31,000
Airport Fund	234,000	0	9,000	25,000	25,000	25,000	25,000	65,000	60,000
Federal Grant	889,993	514,993	0	0	99,000	0	0	0	276,000
State Grant	68,366	18,366	29,000	0	5,000	0	0	0	16,000
<b>Total Funding</b>	<b>1,429,375</b>	<b>733,375</b>	<b>38,000</b>	<b>25,000</b>	<b>135,000</b>	<b>25,000</b>	<b>25,000</b>	<b>65,000</b>	<b>383,000</b>

<b>Project Title</b>	<b>Land Acquisition - Airport</b>
<b>Project ID</b>	LAN018
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$0



<b>Project Description</b>	This project is for land acquisition to promote future economic development of the Airport. FY2022- Topflight corner for Runway 20 Viability Zone and 14223 Oak Springs.      FY2023-Acquire Land for Development FY2025-Aquire Land for Development	
<b>Project Assumptions</b>	Pending grant funding approval: 90% FAA share 5% MAA share 5% Local share	
<b>Project Justification</b>	The new Airport Layout Plan will identify land recommended to be acquired to enhance future development of the Airport.	

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b>Project Costs:</b>									
Land Acquisition	15,841,000	2,507,000	0	2,222,000	5,556,000	0	5,556,000	0	0
<b>Total Cost</b>	<b>15,841,000</b>	<b>2,507,000</b>	<b>0</b>	<b>2,222,000</b>	<b>5,556,000</b>	<b>0</b>	<b>5,556,000</b>	<b>0</b>	<b>0</b>
<b>Funding Sources:</b>									
General Fund	514,000	125,000	0	111,000	278,000	0	0	0	0
Tax-Supported Bond	278,000	0	0	0	0	0	278,000	0	0
Federal Grant	14,256,000	2,256,000	0	2,000,000	5,000,000	0	5,000,000	0	0
State Grant	793,000	126,000	0	111,000	278,000	0	278,000	0	0
<b>Total Funding</b>	<b>15,841,000</b>	<b>2,507,000</b>	<b>0</b>	<b>2,222,000</b>	<b>5,556,000</b>	<b>0</b>	<b>5,556,000</b>	<b>0</b>	<b>0</b>

<b>Project Title</b>	<b>Wildlife Assessment</b>
<b>Project ID</b>	STY023
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$0



**Project Description** Contract firm to conduct Wildlife Hazard Assessment

**Project Assumptions** Grant funding approved:  
90% FAA share  
5% State share  
5% Local share  
Entitlement Funds will be used

**Project Justification** The FAA requires airport sponsors to maintain a safe operating environment, which includes conducting Wildlife Hazard Assessments when deemed necessary by an inspector.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Management	100,000	0	100,000	0	0	0	0	0	0
<b>Total Cost</b>	<b>100,000</b>	<b>0</b>	<b>100,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
Capital Reserve - General	5,000	0	5,000	0	0	0	0	0	0
Federal Grant	90,000	0	90,000	0	0	0	0	0	0
State Grant	5,000	0	5,000	0	0	0	0	0	0
<b>Total Funding</b>	<b>100,000</b>	<b>0</b>	<b>100,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>



**Project Title**      **Proposed Taxiway S****Full-time Employees**      0**Operating Costs**      \$0**Project Description**      The project consists of the design and construction of a new taxiway to connect Runway 27 run-up area to lands North of the Airport.**Project Assumptions**      Pending grant funding approval:  
90% FAA share  
5% MAA share  
5% Local share**Project Justification**      Land is identified for future development of the Airport. A connector taxiway is required to provide access to that land.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Engineering and Design	1,180,000	0	0	0	0	0	0	1,180,000	0
<b>Total Cost</b>	<b>1,180,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1,180,000</b>	<b>0</b>
<b>Funding Sources:</b>									
General Fund	59,000	0	0	0	0	0	0	59,000	0
Federal Grant	1,062,000	0	0	0	0	0	0	1,062,000	0
State Grant	59,000	0	0	0	0	0	0	59,000	0
<b>Total Funding</b>	<b>1,180,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1,180,000</b>	<b>0</b>

**Project Title**      **Runway 9 MALSR****Full-time Employees**      0**Operating Costs**      \$0**Project Description**      This project will design, construct, and commission a medium-intensity approach lighting system with a runway alignment indicator (MALSR) for Runway 9.**Project Assumptions**      Pending grant funding approval:  
90% FAA share  
5% MAA share  
5% Local share**Project Justification**      The installation of a MALSR system for runway 9 provides for safety during flight (landing); lowers approach minimums and increases the potential to increase flights during inclement weather.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
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**Project Costs:**

Construction	1,240,000	0	0	0	0	0	0	0	1,240,000
Engineering and Design	244,000	0	0	0	0	0	0	0	244,000
<b>Total Cost</b>	<b>1,484,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1,484,000</b>

**Funding Sources:**

General Fund	74,000	0	0	0	0	0	0	0	74,000
Federal Grant	1,336,000	0	0	0	0	0	0	0	1,336,000
State Grant	74,000	0	0	0	0	0	0	0	74,000
<b>Total Funding</b>	<b>1,484,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1,484,000</b>

	Prior							
Total	Appr.	2021	2022	2023	2024	2025	2026	Future
1,700,000	0	0	0	0	0	1,700,000	0	0
300,000	0	0	0	0	300,000	0	0	0
<b>2,000,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>300,000</b>	<b>1,700,000</b>	<b>0</b>	<b>0</b>
100,000	0	0	0	0	15,000	85,000	0	0
1,800,000	0	0	0	0	270,000	1,530,000	0	0
100,000	0	0	0	0	15,000	85,000	0	0
<b>2,000,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>300,000</b>	<b>1,700,000</b>	<b>0</b>	<b>0</b>

**Project Title**      **Taxiway F Rehabilitation****Full-time Employees**      0**Operating Costs**      \$0**Project Description**      Project involves the full-length rehabilitation of Taxiway F including replacement of existing light fixture with LED technology**Project Assumptions**      Pending grant funding approval:  
90% FAA share  
5% MAA share  
5% Local share**Project Justification**      Near the end of its useful life and will be ready for rehabilitation.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	6,000,000	0	0	0	0	6,000,000	0	0	0
Engineering and Design	300,000	0	0	0	300,000	0	0	0	0
<b>Total Cost</b>	<b>6,300,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>300,000</b>	<b>6,000,000</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>Funding Sources:</b>									
General Fund	315,000	0	0	0	15,000	300,000	0	0	0
Federal Grant	5,670,000	0	0	0	270,000	5,400,000	0	0	0
State Grant	315,000	0	0	0	15,000	300,000	0	0	0
<b>Total Funding</b>	<b>6,300,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>300,000</b>	<b>6,000,000</b>	<b>0</b>	<b>0</b>	<b>0</b>

**Project Title                      Taxiway G Rehabilitation****Full-time Employees**        0**Operating Costs**             \$0

**Project Description**        Project involves the full-length rehabilitation of Taxiway G including replacement of existing light fixture with LED technology.

**Project Assumptions**      Pending grant funding approval:  
    90% FAA share  
    5% MAA share  
    5% Local share

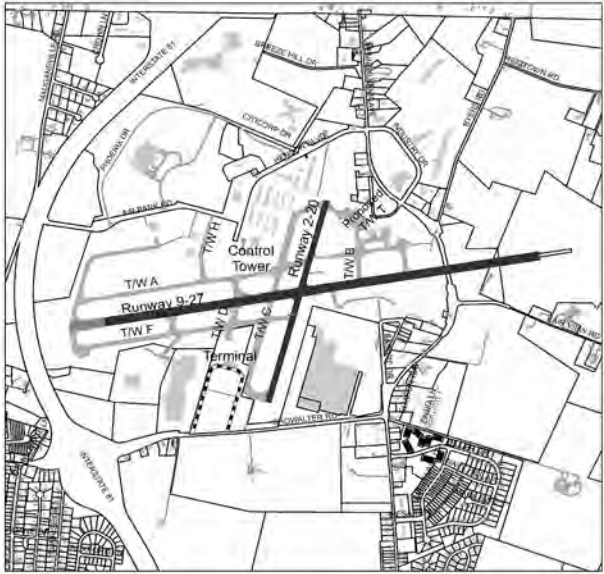
**Project Justification**        The last rehabilitation of the taxiway was completed in 2008. This will make the existing surface approximately 20 years old in 2027 and it will have met the end of its useful life and will be ready for rehabilitation.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	1,440,000	0	0	0	0	0	0	0	1,440,000
Engineering and Design	480,000	0	0	0	0	0	0	0	480,000
<b>Total Cost</b>	<b>1,920,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1,920,000</b>
<b><u>Funding Sources:</u></b>									
General Fund	96,000	0	0	0	0	0	0	0	96,000
Federal Grant	1,728,000	0	0	0	0	0	0	0	1,728,000
State Grant	96,000	0	0	0	0	0	0	0	96,000
<b>Total Funding</b>	<b>1,920,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1,920,000</b>

**Project Title** Taxiway H Rehabilitation

**Full-time Employees** 0

**Operating Costs** \$0



**Project Description** Taxiway H will be in need of total rehabilitation.

**Project Assumptions** Pending grant funding approval:  
90% FAA share  
5% MAA share  
5% Local share

**Project Justification** The project will involve sub-surface repairs, milling, overlay, painting and new LED lighting installation.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Engineering and Design	300,000	0	0	0	0	0	0	0	300,000
<b>Total Cost</b>	<b>300,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>300,000</b>
<b>Funding Sources:</b>									
General Fund	15,000	0	0	0	0	0	0	0	15,000
Federal Grant	270,000	0	0	0	0	0	0	0	270,000
State Grant	15,000	0	0	0	0	0	0	0	15,000
<b>Total Funding</b>	<b>300,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>300,000</b>

# Bridges

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*Bridges*  
*Capital Improvement Ten Year Summary*  
*Fiscal Year 2021 – 2030*

Page	Project	Total	Prior Appr.	Budget Year		Ten Year Capital Program					Future
				FY 2021	FY 2022	FY 2023	FY 2024	FY 2025	FY 2026		
Project Costs											
Bridges											
60	Bridge Inspection and Inventory	699,250	287,250	0	22,000	0	171,000	0	24,000	195,000	
61	Roxbury Road Bridge W5372	3,892,077	2,892,077	0	500,000	500,000	0	0	0	0	
62	Bridge Scour Repairs	513,184	31,184	0	0	0	0	0	232,000	250,000	
63	Cleaning & Painting of Steel Bridges	408,000	108,000	0	150,000	150,000	0	0	0	0	
64	Halfway Boulevard Bridges W0912	4,489,000	575,000	1,132,000	1,182,000	1,600,000	0	0	0	0	
65	Keefer Road Bridge 15/20	231,000	85,000	146,000	0	0	0	0	0	0	
66	Frog Eye Road Culvert 11/06	652,000	0	652,000	0	0	0	0	0	0	
67	Appletown Road Bridge W2184	474,000	0	0	0	0	0	0	0	474,000	
68	Ashton Road Culvert 04/06	30,000	0	0	0	0	0	0	0	30,000	
69	Bowie Road Culvert	305,000	0	0	0	0	0	0	0	305,000	
70	Burnside Bridge Road Culvert 01/03	329,000	0	0	0	0	0	114,000	215,000	0	
71	Draper Road Culvert 04/07	428,000	0	0	0	0	0	0	0	428,000	
72	Greenspring Furnace Road Culvert 15/15	398,000	0	0	188,000	210,000	0	0	0	0	
73	Gruber Road Bridge 04/10	10,000	0	0	0	0	0	0	0	10,000	
74	Harpers Ferry Road Culvert 11/02	541,000	0	0	0	0	0	0	82,000	459,000	
75	Henline Road Culvert 05/05	465,000	0	0	0	0	0	0	0	465,000	
76	Hoffman's Inn Road Culvert 05/06	313,000	0	0	0	0	0	0	0	313,000	
77	Kretsinger Road Culvert 14/01	316,000	0	0	0	137,000	179,000	0	0	0	
78	Lanes Road Culvert 15/12	317,000	0	0	0	0	32,000	285,000	0	0	
79	Long Hollow Road Culvert 05/07	316,000	0	0	0	0	0	0	66,000	250,000	
80	Mooreville Road Culvert 15/21	355,000	0	0	0	0	0	0	0	355,000	
81	Remsburg Road Culvert	287,000	0	0	0	0	0	119,000	168,000	0	
82	Rinehart Road Culvert 14/03	332,000	0	0	0	136,000	196,000	0	0	0	
83	Stone Masonry Bridge Repairs	270,000	0	0	0	0	0	0	0	270,000	
84	Willow Road Culvert 05/10	323,000	0	0	0	0	0	0	0	323,000	
85	Yarrowsburg Road Bridge W6191	2,102,000	0	0	0	0	0	0	0	2,102,000	
Bridges Total		18,795,511	3,978,511	1,930,000	2,042,000	2,733,000	578,000	518,000	787,000	6,229,000	
Funding Sources											
General Fund		1,849,761	385,761	0	172,000	150,000	171,000	0	256,000	715,000	
Tax-Supported Bond		8,974,000	398,000	1,009,000	926,000	1,303,000	407,000	518,000	531,000	3,882,000	
Capital Reserve - General		175,000	175,000	0	0	0	0	0	0	0	
Federal Grant		7,745,000	2,968,000	921,000	944,000	1,280,000	0	0	0	1,632,000	
Contributions		51,750	51,750	0	0	0	0	0	0	0	
		18,795,511	3,978,511	1,930,000	2,042,000	2,733,000	578,000	518,000	787,000	6,229,000	

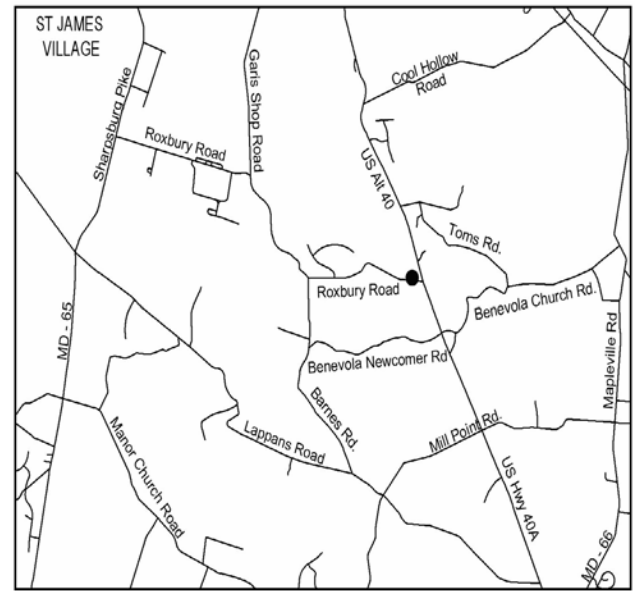


Project Title	Bridge Inspection & Inventory
Project ID	BRG002
Full-time Employees	0
Operating Costs	\$1,080

Project Description	This project provides an ongoing bridge inspection program for both major (bridges greater than 20 feet in length) and minor (bridges greater than 6 but less than 20 feet in length) structures. Federal regulations govern the major structures inspection program.
Project Assumptions	Federal aid bridge funds cover all of the costs for the major bridges. They are not shown herein since they are paid directly by the State Highway Administration to the consultant, and do not pass through the County's records. Only local funds required are shown below. Minor structures are fully locally funded as they are not eligible for any federal aid money.
Project Justification	Various portions of this project are required by State and Federal regulations, while other portions are done as needed to assure public safety and to minimize maintenance costs. This project supports the infrastructure needs of the County under its bridge infrastructure program.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	51,750	51,750	0	0	0	0	0	0	0
Engineering and Design	31,500	31,500	0	0	0	0	0	0	0
Other	616,000	204,000	0	22,000	0	171,000	0	24,000	195,000
Total Cost	699,250	287,250	0	22,000	0	171,000	0	24,000	195,000
<b>Funding Sources:</b>									
General Fund	472,500	60,500	0	22,000	0	171,000	0	24,000	195,000
Capital Reserve - General	175,000	175,000	0	0	0	0	0	0	0
Contributions	51,750	51,750	0	0	0	0	0	0	0
Total Funding	699,250	287,250	0	22,000	0	171,000	0	24,000	195,000

<b>Project Title</b>	<b>Old Roxbury Road Bridge W5372</b>
<b>Project ID</b>	BRG020
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$0



<b>Project Description</b>	This project involves the replacement of an existing one lane, two span bridge with a two-lane multiple span bridge. The existing bridge has flooding issues and is structurally deficient and functionally obsolete.
<b>Project Assumptions</b>	The project is proposed as a Federal Aid project with 80/20 cost share for construction.
<b>Project Justification</b>	The project is needed to replace a structure that has exceeded its useful life and to improve safety. Due to the structure type, no practical long-term repair options exist. The Bridge Sufficiency Rating is 13.6 out of a maximum possible score of 100.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	2,675,700	1,675,700	0	500,000	500,000	0	0	0	0
Engineering and Design	685,177	685,177	0	0	0	0	0	0	0
Inspection	250,200	250,200	0	0	0	0	0	0	0
Land Acquisition	281,000	281,000	0	0	0	0	0	0	0
<b>Total Cost</b>	<b>3,892,077</b>	<b>2,892,077</b>	<b>0</b>	<b>500,000</b>	<b>500,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
General Fund	156,077	156,077	0	0	0	0	0	0	0
Tax-Supported Bond	1,228,000	228,000	0	500,000	500,000	0	0	0	0
Federal Grant	2,508,000	2,508,000	0	0	0	0	0	0	0
<b>Total Funding</b>	<b>3,892,077</b>	<b>2,892,077</b>	<b>0</b>	<b>500,000</b>	<b>500,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

Project Title	Bridge Scour Repairs
Project ID	BRG081
Full-time Employees	0
Operating Costs	\$0

Project Description	The project will perform bridge scour countermeasures on several bridges throughout the County. The repairs will include either providing concrete collars, grout bags, or riprap to stabilize the foundations of the bridges.
Project Assumptions	The major bridge inspection program revealed the need to address this issue and documented the bridges needing the repairs.
Project Justification	The project is needed to extend the useful life of the structures in a cost-effective manner and to maintain the structures in a serviceable condition. Failure to address these repairs could result in closure of the structures to traffic. Recent changes in the federal bridge inspection program required this activity.

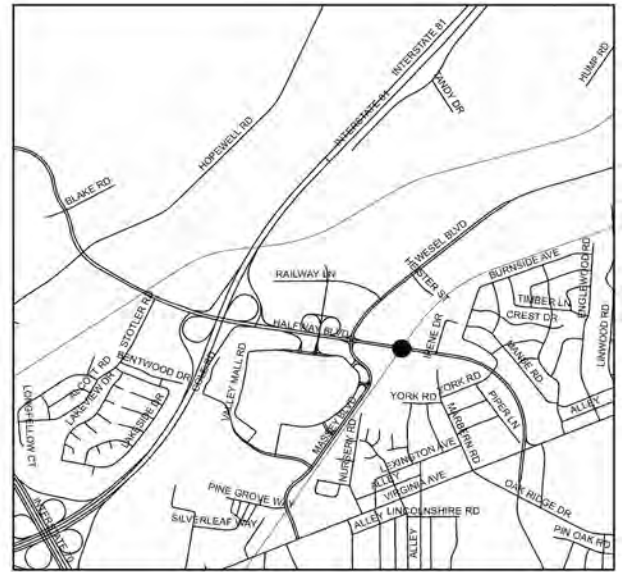
	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	513,184	31,184	0	0	0	0	0	232,000	250,000
Total Cost	513,184	31,184	0	0	0	0	0	232,000	250,000
<b>Funding Sources:</b>									
General Fund	513,184	31,184	0	0	0	0	0	232,000	250,000
Total Funding	513,184	31,184	0	0	0	0	0	232,000	250,000

Project Title	Cleaning and Painting of Steel Bridges
Project ID	BRG082
Full-time Employees	0
Operating Costs	\$0

Project Description	The project consists of cleaning and painting various steel beam bridges throughout the County.
Project Justification	The project is essential to maintain and extend the useful life of the structure.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	367,000	67,000	0	150,000	150,000	0	0	0	0
Inspection	41,000	41,000	0	0	0	0	0	0	0
Total Cost	408,000	108,000	0	150,000	150,000	0	0	0	0
<b>Funding Sources:</b>									
General Fund	408,000	108,000	0	150,000	150,000	0	0	0	0
Total Funding	408,000	108,000	0	150,000	150,000	0	0	0	0

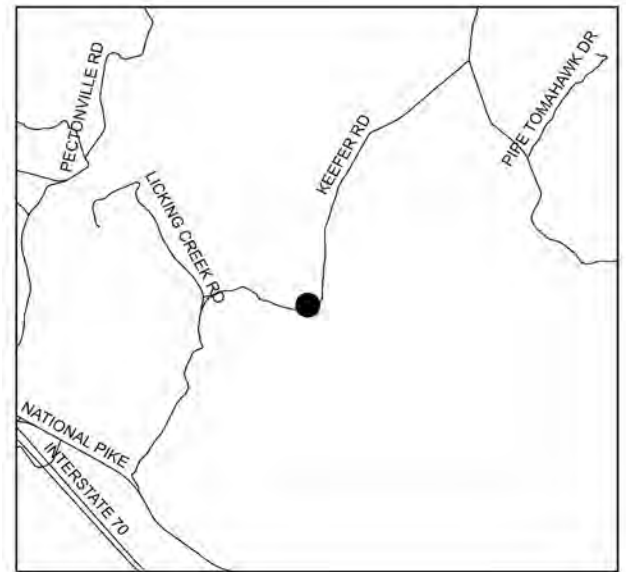
<b>Project Title</b>	<b>Halfway Boulevard Bridges W0912</b>
<b>Project ID</b>	BRG083
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$0



<b>Project Description</b>	The project involves repairing the eastbound and westbound bridges of Halfway Boulevard over the Norfolk Southern railroad (ADC Map 20, H-8). The project will include cleaning and painting of steel beams, replacement of bearings and expansion joints, concrete repairs to the substructure and superstructure, traffic barrier improvements, and minor paving work.
<b>Project Assumptions</b>	The project is proposed as a Federal Aid project with 80/20 cost share for construction.
<b>Project Justification</b>	The project is essential to maintain and extend the useful life of the structure. The Bridge Sufficiency Rating is 49.4 out of a maximum possible score of 100.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	3,489,000	0	1,007,000	882,000	1,600,000	0	0	0	0
Engineering and Design	700,000	575,000	125,000	0	0	0	0	0	0
Inspection	300,000	0	0	300,000	0	0	0	0	0
<b>Total Cost</b>	<b>4,489,000</b>	<b>575,000</b>	<b>1,132,000</b>	<b>1,182,000</b>	<b>1,600,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
Tax-Supported Bond	884,000	115,000	211,000	238,000	320,000	0	0	0	0
Federal Grant	3,605,000	460,000	921,000	944,000	1,280,000	0	0	0	0
<b>Total Funding</b>	<b>4,489,000</b>	<b>575,000</b>	<b>1,132,000</b>	<b>1,182,000</b>	<b>1,600,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

<b>Project Title</b>	<b>Keefer Road Bridge 15/20</b>
<b>Project ID</b>	BRG085
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$0



<b>Project Description</b>	This project is located in the 12800 block of Keefer Road (ADC Map 16, Grid J-1). The project will replace a steel stringer/timber deck with a single lane concrete slab bridge and install an appropriate traffic barrier.
<b>Project Assumptions</b>	The project is to be done in conjunction with Mt. Tabor Road Bridge 13/03.
<b>Project Justification</b>	The project is needed to extend the useful life of this structure in a cost-effective manner and improve vehicle safety.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	195,000	49,000	146,000	0	0	0	0	0	0
Inspection	26,000	26,000	0	0	0	0	0	0	0
Land Acquisition	10,000	10,000	0	0	0	0	0	0	0
<b>Total Cost</b>	<b>231,000</b>	<b>85,000</b>	<b>146,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
General Fund	30,000	30,000	0	0	0	0	0	0	0
Tax-Supported Bond	201,000	55,000	146,000	0	0	0	0	0	0
<b>Total Funding</b>	<b>231,000</b>	<b>85,000</b>	<b>146,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

<b>Project Title</b>	<b>Frog Eye Road Culvert 11/06</b>
<b>Project ID</b>	BRG087
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$0



<b>Project Description</b>	This project is located in the 19800 block of Frog Eye Road (ADC Map 37, Grid D-4). The project will replace a single lane steel stringer/open steel grid deck with a two-cell box culvert with headwalls. The project will include realignment of the road, use of existing bridge for access during construction, and the installation of an appropriate traffic barrier.
<b>Project Assumptions</b>	The project assumes realignment of the existing roadway and use of the existing bridge during construction.
<b>Project Justification</b>	The project is needed to replace a structure that is near the end of its useful life and to improve vehicle safety.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Construction	568,000	0	568,000	0	0	0	0	0	0
Inspection	50,000	0	50,000	0	0	0	0	0	0
Land Acquisition	11,000	0	11,000	0	0	0	0	0	0
Utilities	23,000	0	23,000	0	0	0	0	0	0
<b>Total Cost</b>	<b>652,000</b>	<b>0</b>	<b>652,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
Tax-Supported Bond	652,000	0	652,000	0	0	0	0	0	0
<b>Total Funding</b>	<b>652,000</b>	<b>0</b>	<b>652,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

**Project Title**                      **Appletown Road Bridge W2184**

**Full-time Employees**            0

**Operating Costs**                  \$0



**Project Description**            This project is located in the 6100 block of Appletown Road (ADC Map 32, E-8). The project will replace the existing concrete bridge with a concrete box culvert and headwalls.

**Project Justification**           This project is needed to replace a structure that is near the end of its useful life. The Bridge Sufficiency Rating is 38.0 out of a maximum possible score of 100. The bridge is classified as structurally deficient and scour critical.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	405,000	0	0	0	0	0	0	0	405,000
Inspection	44,000	0	0	0	0	0	0	0	44,000
Land Acquisition	10,000	0	0	0	0	0	0	0	10,000
Utilities	15,000	0	0	0	0	0	0	0	15,000
<b>Total Cost</b>	<b>474,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>474,000</b>
<b><u>Funding Sources:</u></b>									
Tax-Supported Bond	474,000	0	0	0	0	0	0	0	474,000
<b>Total Funding</b>	<b>474,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>474,000</b>



Project Title

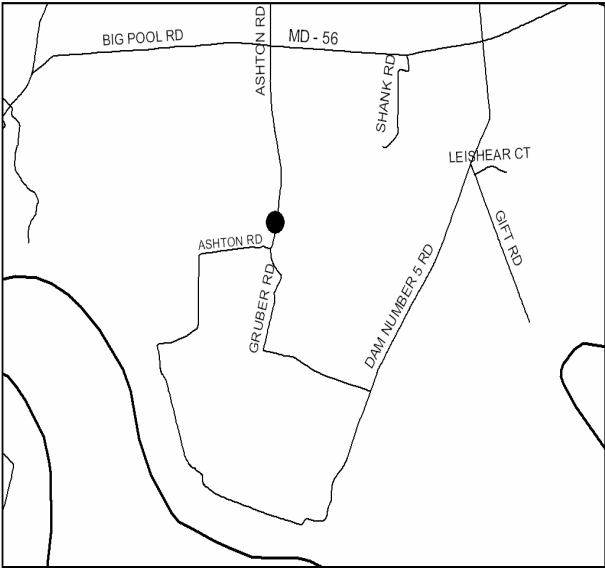
Ashton Road Culvert 04/06

Full-time Employees

0

Operating Costs

\$0



Project Description

This project is located in the 11200 block of Ashton Road (ADC Map 18, Grid E-9). The project will replace the concrete slab with a concrete box culvert with headwalls. The traffic barrier will be upgraded to improve safety.

Project Assumptions

The project is to be done in conjunction with Gruber Road Bridge 04/10.

Project Justification

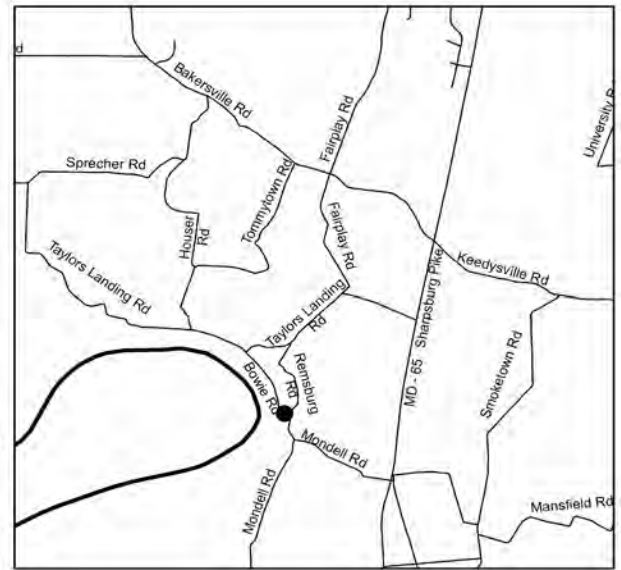
The project is needed to replace a structure that is near the end of its useful life and improve vehicle safety.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Land Acquisition	6,000	0	0	0	0	0	0	0	6,000
Utilities	24,000	0	0	0	0	0	0	0	24,000
<b>Total Cost</b>	<b>30,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>30,000</b>
<b>Funding Sources:</b>									
Tax-Supported Bond	30,000	0	0	0	0	0	0	0	30,000
<b>Total Funding</b>	<b>30,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>30,000</b>

**Project Title**                      **Bowie Road Culvert**

**Full-time Employees**            0

**Operating Costs**                    \$0



**Project Description**            This project is located in the 6600 Block of Bowie Road (ADC Map 30, Grid H-6). The project will replace the culverts with a precast concrete box culvert and headwalls.

**Project Assumptions**           The project will be done in conjunction with Rensburg Road Culvert.

**Project Justification**           The project is needed to replace a structure that is near the end of its useful life and to increase the structure size to reduce frequency of roadway overtopping.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	236,000	0	0	0	0	0	0	0	236,000
Inspection	34,000	0	0	0	0	0	0	0	34,000
Land Acquisition	12,000	0	0	0	0	0	0	0	12,000
Utilities	23,000	0	0	0	0	0	0	0	23,000
<b>Total Cost</b>	<b>305,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>305,000</b>
<b><u>Funding Sources:</u></b>									
Tax-Supported Bond	305,000	0	0	0	0	0	0	0	305,000
<b>Total Funding</b>	<b>305,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>305,000</b>

Project Title

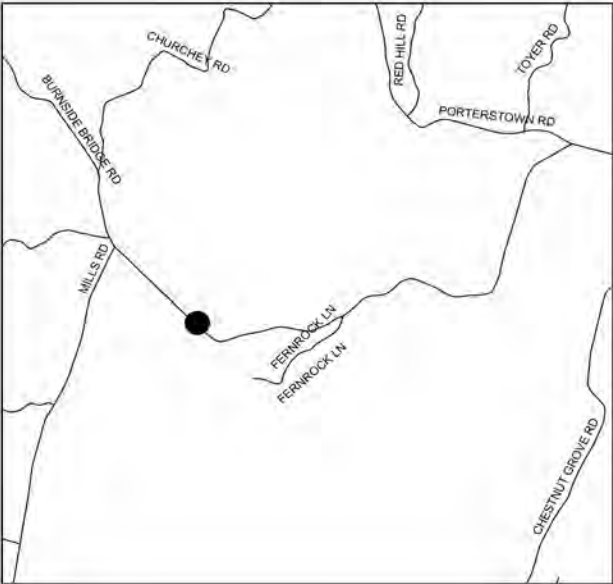
Burnside Bridge Road Culvert 01/03

Full-time Employees

0

Operating Costs

\$0



Project Description

This project is located in the 18900 block of Burnside Bridge Road (ADC Map CIP Transit Fund, Grid E-4).  
The project will replace the concrete slab with a precast concrete box culvert and headwalls.

Project Justification

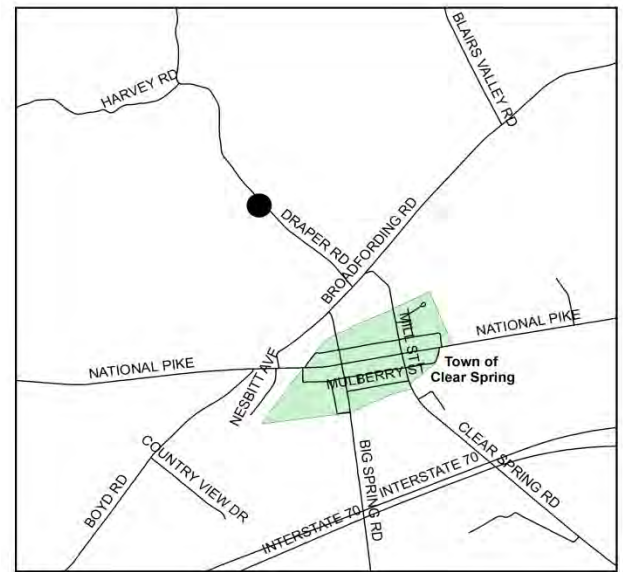
The project is needed to replace a structure that is near the end of its useful life and to improve safety.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	264,000	0	0	0	0	0	81,000	183,000	0
Inspection	32,000	0	0	0	0	0	0	32,000	0
Land Acquisition	11,000	0	0	0	0	0	11,000	0	0
Utilities	22,000	0	0	0	0	0	22,000	0	0
Total Cost	329,000	0	0	0	0	0	114,000	215,000	0
<b>Funding Sources:</b>									
Tax-Supported Bond	329,000	0	0	0	0	0	114,000	215,000	0
Total Funding	329,000	0	0	0	0	0	114,000	215,000	0

**Project Title**                      **Draper Road Culvert 04/07**

**Full-time Employees**            0

**Operating Costs**                  \$0



**Project Description**            This project is located in the 12700 Block of Draper Road (ADC Map 18, Grid B-1). The project will replace the concrete slab with a new precast concrete box culvert.

**Project Justification**           The project is needed to replace a structure that is near the end of its useful life. Due to the structure type, no practical long-term repair options exist. Traffic barrier upgrades will also improve traffic safety.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Construction	347,000	0	0	0	0	0	0	0	347,000
Inspection	45,000	0	0	0	0	0	0	0	45,000
Land Acquisition	12,000	0	0	0	0	0	0	0	12,000
Utilities	24,000	0	0	0	0	0	0	0	24,000
<b>Total Cost</b>	<b>428,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>428,000</b>
<b><u>Funding Sources:</u></b>									
Tax-Supported Bond	428,000	0	0	0	0	0	0	0	428,000
<b>Total Funding</b>	<b>428,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>428,000</b>

Project Title

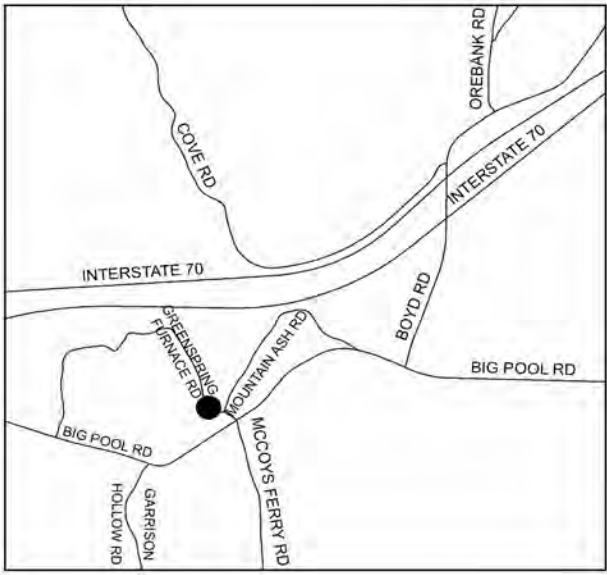
Greenspring Furnace Road Culvert 15/15

Full-time Employees

0

Operating Costs

\$0



Project Description

This project is located on Greenspring Furnace Road (ADC Map 17, Grid G-8). The project will replace the single lane concrete slab with a two-cell concrete box culvert with headwalls.

Project Assumptions

The project assumes temporary roadway stream/culvert crossing during construction.

Project Justification

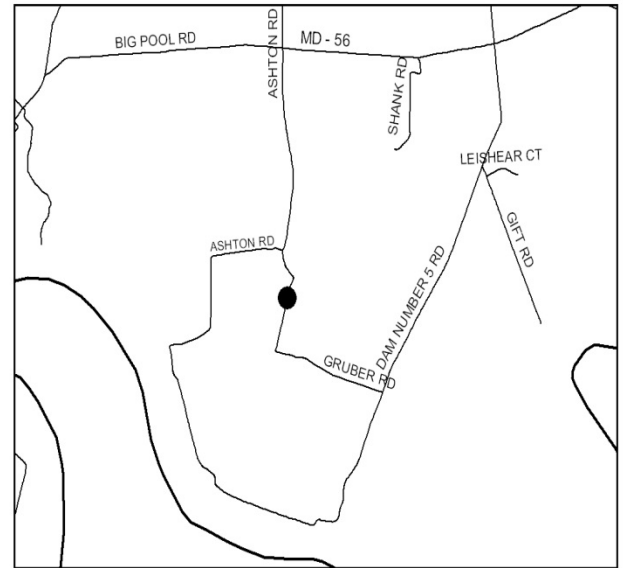
The project is needed to replace a structure that is near the end of its useful life and to improve vehicle safety.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	335,000	0	0	156,000	179,000	0	0	0	0
Inspection	31,000	0	0	0	31,000	0	0	0	0
Land Acquisition	11,000	0	0	11,000	0	0	0	0	0
Utilities	21,000	0	0	21,000	0	0	0	0	0
Total Cost	398,000	0	0	188,000	210,000	0	0	0	0
<b>Funding Sources:</b>									
Tax-Supported Bond	398,000	0	0	188,000	210,000	0	0	0	0
Total Funding	398,000	0	0	188,000	210,000	0	0	0	0

**Project Title**                      **Gruber Road Bridge 04/10**

**Full-time Employees**            0

**Operating Costs**                  \$0



**Project Description**            The project is located in the 13200 block of Gruber Road (ADC Map 18, Grid E-9). The project will replace the concrete slab with a new concrete superstructure. Additional repairs include substructure concrete repairs and scour countermeasures.

**Project Assumptions**           The project is to be done in conjunction with Ashton Road Culvert 04-06.

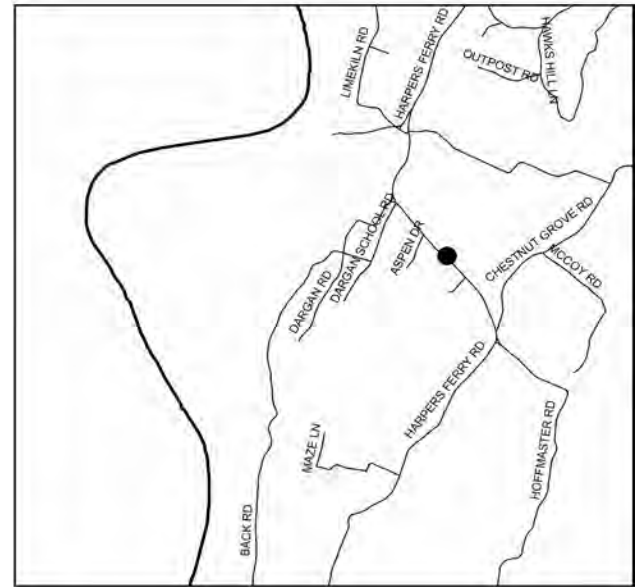
**Project Justification**           The project is needed to replace a superstructure that is near the end of its useful life and improve vehicle safety.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Land Acquisition	6,000	0	0	0	0	0	0	0	6,000
Utilities	4,000	0	0	0	0	0	0	0	4,000
<b>Total Cost</b>	<b>10,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>10,000</b>
<b><u>Funding Sources:</u></b>									
Tax-Supported Bond	10,000	0	0	0	0	0	0	0	10,000
<b>Total Funding</b>	<b>10,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>10,000</b>

**Project Title**                      **Harpers Ferry Road Culvert 11/02**

**Full-time Employees**            0

**Operating Costs**                   \$0



**Project Description**            This project is located in the 2300 block of Harpers Ferry Road (ADC Map 36, Grid G-1). The project will replace the concrete slab with a two-cell concrete box culvert and headwalls. The project will include installation of an appropriate traffic barrier.

**Project Justification**            The project is needed to replace a structure that is near the end of its useful life and to improve vehicle safety.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	468,000	0	0	0	0	0	0	49,000	419,000
Inspection	40,000	0	0	0	0	0	0	0	40,000
Land Acquisition	11,000	0	0	0	0	0	0	11,000	0
Utilities	22,000	0	0	0	0	0	0	22,000	0
<b>Total Cost</b>	<b>541,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>82,000</b>	<b>459,000</b>
<b><u>Funding Sources:</u></b>									
Tax-Supported Bond	541,000	0	0	0	0	0	0	82,000	459,000
<b>Total Funding</b>	<b>541,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>82,000</b>	<b>459,000</b>

Project Title

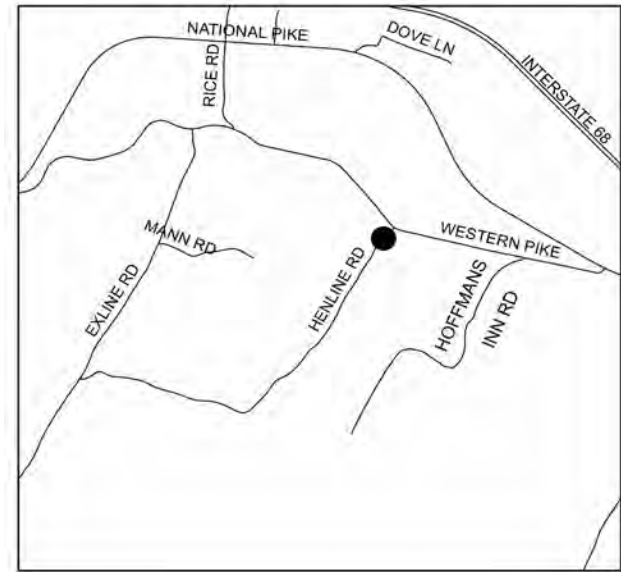
Henline Road Culvert 05/05

Full-time Employees

0

Operating Costs

\$0



Project Description

This project is located in the 3200 block of Henline Road (ADC Map 2, Grid C-8). The project will replace the concrete slab with a concrete box culvert and headwalls. The project will provide for the installation of an appropriate traffic barrier.

Project Assumptions

The project is to be done in conjunction with Longmeadow Road Culvert 05/07 and Hoffman's Inn Road Culvert 05/06.

Project Justification

The project is needed to replace a structure that is near the end of its useful life and to improve vehicle safety.

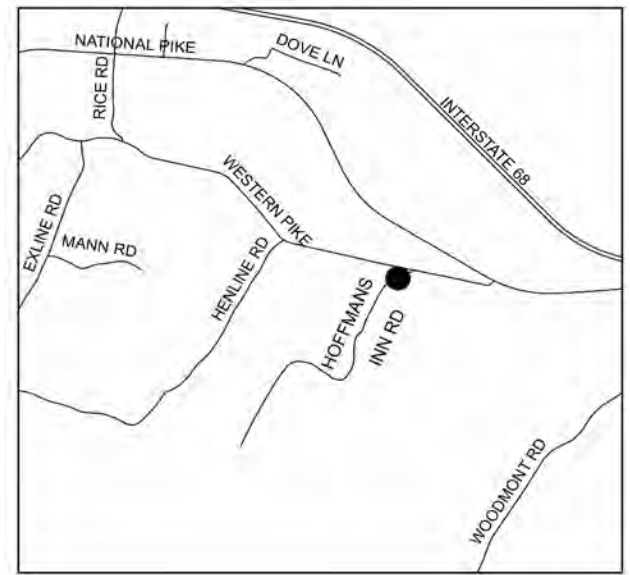
	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	389,000	0	0	0	0	0	0	0	389,000
Inspection	42,000	0	0	0	0	0	0	0	42,000
Land Acquisition	11,000	0	0	0	0	0	0	0	11,000
Utilities	23,000	0	0	0	0	0	0	0	23,000
Total Cost	465,000	0	0	0	0	0	0	0	465,000
<b>Funding Sources:</b>									
Tax-Supported Bond	465,000	0	0	0	0	0	0	0	465,000
Total Funding	465,000	0	0	0	0	0	0	0	465,000



**Project Title** Hoffman's Inn Road Culvert 05/06

**Full-time Employees** 0

**Operating Costs** \$0



**Project Description** This project is located in the 2500 block of Hoffman's Inn Road (ADC Map 2, Grid D-8). The project will replace the concrete slab with a concrete box culvert and headwalls. The project will provide for the installation of an appropriate traffic barrier.

**Project Assumptions** The project is to be done in conjunction with Long Hollow Road Culvert 05/07 and Henline Road Culvert 05/05.

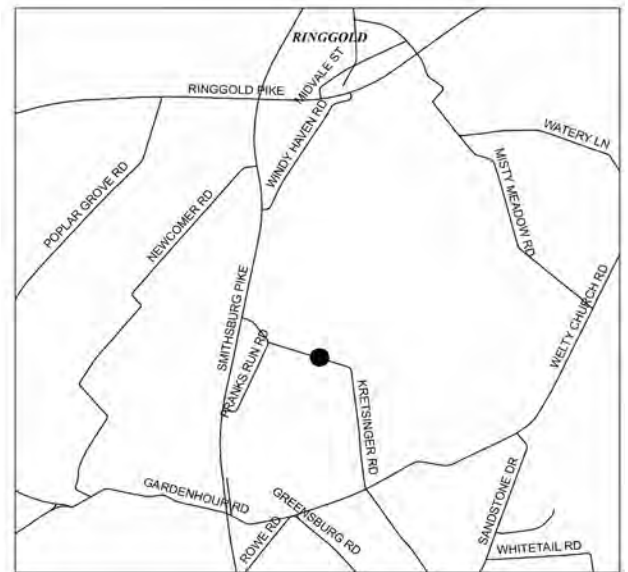
**Project Justification** The project is needed to replace a structure that is near the end of its useful life and to improve vehicle safety.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	247,000	0	0	0	0	0	0	0	247,000
Inspection	32,000	0	0	0	0	0	0	0	32,000
Land Acquisition	11,000	0	0	0	0	0	0	0	11,000
Utilities	23,000	0	0	0	0	0	0	0	23,000
<b>Total Cost</b>	<b>313,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>313,000</b>
<b>Funding Sources:</b>									
Tax-Supported Bond	313,000	0	0	0	0	0	0	0	313,000
<b>Total Funding</b>	<b>313,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>313,000</b>

**Project Title**                      **Kretsinger Road Culvert 14/01**

**Full-time Employees**            0

**Operating Costs**                  \$0



**Project Description**            This project is located in the 13600 block of Kretsinger Road (ADC Map 12, Grid G-8). The project will replace the concrete slab bridge with a two-cell concrete box culvert and headwalls. The project will install an appropriate traffic barrier.

**Project Assumptions**           The project is to be done in conjunction with Rinehart Road culvert 14/03.

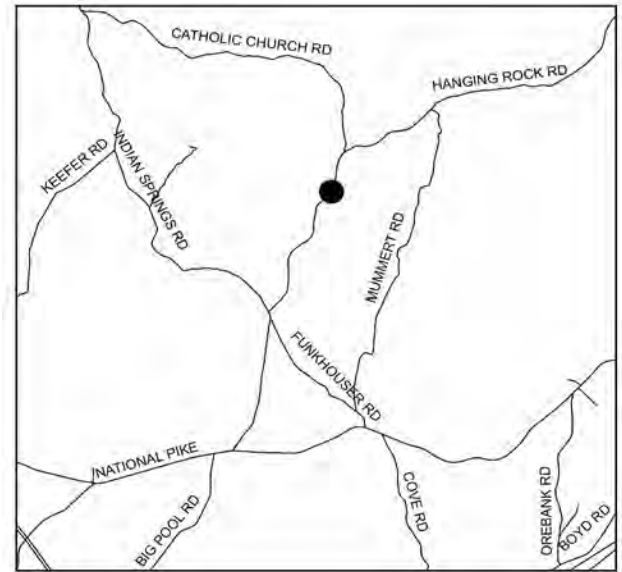
**Project Justification**           The project is needed to replace a structure that is near the end of its useful life and improve vehicle safety.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	254,000	0	0	0	106,000	148,000	0	0	0
Inspection	31,000	0	0	0	0	31,000	0	0	0
Land Acquisition	10,000	0	0	0	10,000	0	0	0	0
Utilities	21,000	0	0	0	21,000	0	0	0	0
<b>Total Cost</b>	<b>316,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>137,000</b>	<b>179,000</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
Tax-Supported Bond	316,000	0	0	0	137,000	179,000	0	0	0
<b>Total Funding</b>	<b>316,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>137,000</b>	<b>179,000</b>	<b>0</b>	<b>0</b>	<b>0</b>

**Project Title** Lanes Road Culvert 15/12

**Full-time Employees** 0

**Operating Costs** \$0



**Project Description** This project is located in the 12900 block of Lanes Road (ADC Map 6, D-12). The project will replace the deteriorated two-lane concrete slab bridge with a precast box culvert. An upgraded traffic barrier will be added as well to improve safety.

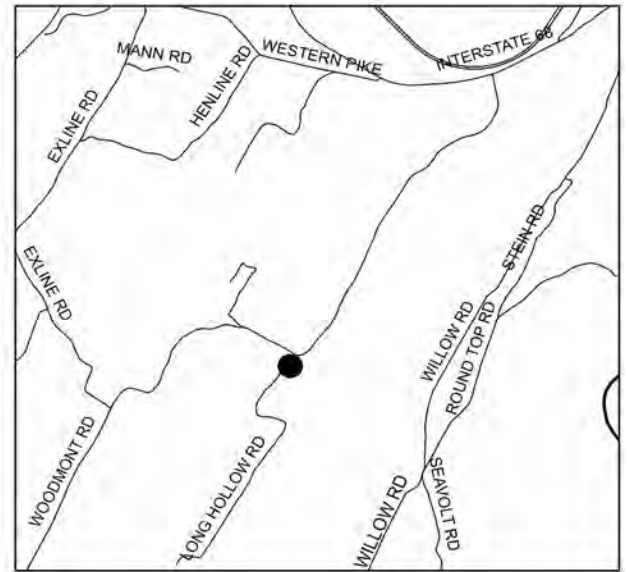
**Project Justification** The structure needs to be replaced due to its advanced deterioration as it is nearing the end of its useful life. Due to the structure type, no practical long-term repair options exist.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	254,000	0	0	0	0	0	254,000	0	0
Inspection	31,000	0	0	0	0	0	31,000	0	0
Land Acquisition	11,000	0	0	0	0	11,000	0	0	0
Utilities	21,000	0	0	0	0	21,000	0	0	0
<b>Total Cost</b>	<b>317,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>32,000</b>	<b>285,000</b>	<b>0</b>	<b>0</b>
<b>Funding Sources:</b>									
Tax-Supported Bond	317,000	0	0	0	0	32,000	285,000	0	0
<b>Total Funding</b>	<b>317,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>32,000</b>	<b>285,000</b>	<b>0</b>	<b>0</b>

**Project Title**                      **Long Hollow Road Culvert 05/07**

**Full-time Employees**            0

**Operating Costs**                  \$0



**Project Description**            This project is located in the 13000 block of Long Hollow Road (ADC Map 2, Grid D-12). The project will replace the concrete slab with a concrete box culvert and headwalls. The project will also provide for the installation of an appropriate traffic barrier.

**Project Assumptions**           The project is to be done in conjunction with Henline Road Culvert 05/05 and Hoffman's Inn Road 05/06.

**Project Justification**           The project is needed to replace a structure that is near the end of its useful life and to improve vehicle safety.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	249,000	0	0	0	0	0	0	33,000	216,000
Inspection	34,000	0	0	0	0	0	0	0	34,000
Land Acquisition	11,000	0	0	0	0	0	0	11,000	0
Utilities	22,000	0	0	0	0	0	0	22,000	0
<b>Total Cost</b>	<b>316,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>66,000</b>	<b>250,000</b>
<b><u>Funding Sources:</u></b>									
Tax-Supported Bond	316,000	0	0	0	0	0	0	66,000	250,000
<b>Total Funding</b>	<b>316,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>66,000</b>	<b>250,000</b>

Project Title

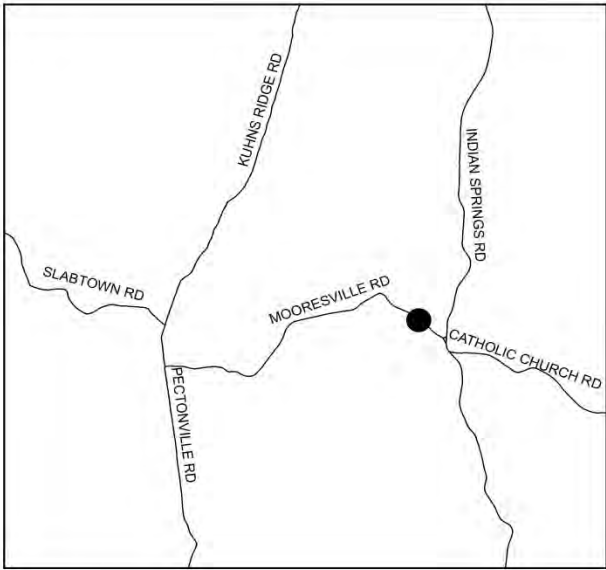
Mooresville Road Culvert 15/21

Full-time Employees

0

Operating Costs

\$0



Project Description

This project is located in the 10200 Block of Mooresville Road (ADC Map 5, Grid K-9). The project will replace a deteriorated multiple corrugated metal pipe culvert with a new precast concrete box culvert.

Project Justification

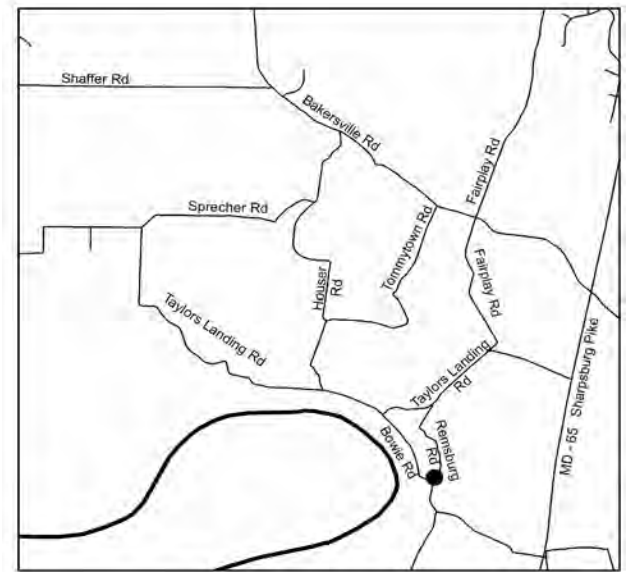
The project is needed to replace a structure that is near the end of its useful life. Due to the structure type, no practical long-term repair options exist. Traffic barrier upgrades will also improve traffic safety.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	277,000	0	0	0	0	0	0	0	277,000
Inspection	42,000	0	0	0	0	0	0	0	42,000
Land Acquisition	12,000	0	0	0	0	0	0	0	12,000
Utilities	24,000	0	0	0	0	0	0	0	24,000
Total Cost	355,000	0	0	0	0	0	0	0	355,000
<b>Funding Sources:</b>									
Tax-Supported Bond	355,000	0	0	0	0	0	0	0	355,000
Total Funding	355,000	0	0	0	0	0	0	0	355,000

**Project Title**                      **Remsburg Road Culvert**

**Full-time Employees**            0

**Operating Costs**                  \$0



**Project Description**            The project is located in the 6600 Block of Remsburg Road (ADC Map 30, Grid H-6). The project will replace the culverts with a precast concrete box culvert and headwalls. The project will raise the existing roadway elevation at crossing to reduce overtopping.

**Project Assumptions**          The project is to be done in conjunction with Bowie Road Culvert.

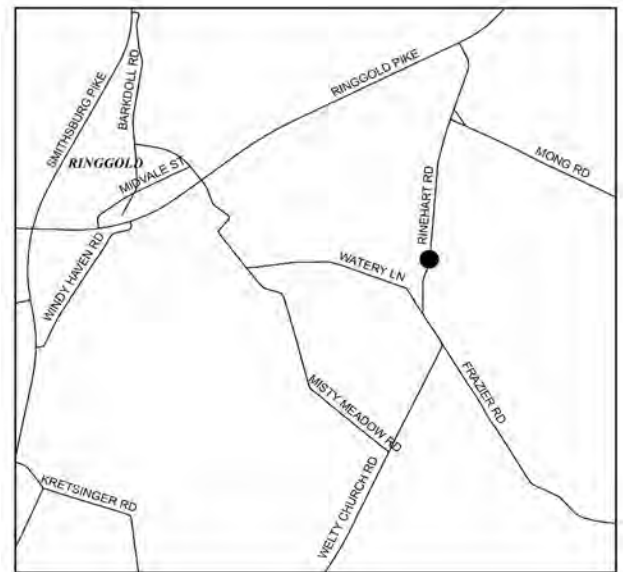
**Project Justification**          The project is needed to replace a structure that is near the end of its useful life and will increase the structure size to reduce the frequency of roadway overtopping.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	219,000	0	0	0	0	0	84,000	135,000	0
Inspection	33,000	0	0	0	0	0	0	33,000	0
Land Acquisition	12,000	0	0	0	0	0	12,000	0	0
Utilities	23,000	0	0	0	0	0	23,000	0	0
<b>Total Cost</b>	<b>287,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>119,000</b>	<b>168,000</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
Tax-Supported Bond	287,000	0	0	0	0	0	119,000	168,000	0
<b>Total Funding</b>	<b>287,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>119,000</b>	<b>168,000</b>	<b>0</b>

**Project Title** Rinehart Road Culvert 14/03

**Full-time Employees** 0

**Operating Costs** \$0



**Project Description** This project is located in the 14300 block of Rinehart Road (ADC Map 12, Grid J-6). The project will replace the concrete slab bridge with a two-cell concrete box culvert and headwalls. The project will install an appropriate traffic barrier.

**Project Assumptions** The project is to be completed with Kretsinger Road culvert 14/01.

**Project Justification** The project is needed to replace a structure that is near the end of its useful life and will improve vehicle safety.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Construction	270,000	0	0	0	105,000	165,000	0	0	0
Inspection	31,000	0	0	0	0	31,000	0	0	0
Land Acquisition	10,000	0	0	0	10,000	0	0	0	0
Utilities	21,000	0	0	0	21,000	0	0	0	0
<b>Total Cost</b>	<b>332,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>136,000</b>	<b>196,000</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
Tax-Supported Bond	332,000	0	0	0	136,000	196,000	0	0	0
<b>Total Funding</b>	<b>332,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>136,000</b>	<b>196,000</b>	<b>0</b>	<b>0</b>	<b>0</b>

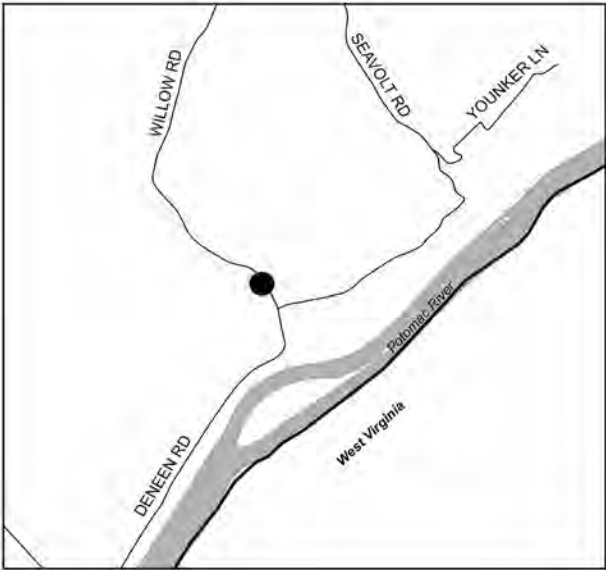
Project Title	Stone Masonry Bridge Repairs
Full-time Employees	0
Operating Costs	\$0

Project Description	This project includes performing repairs to various masonry stone arch minor bridge structures at multiple locations in the county. Repairs include rebuilding stone masonry, repointing mortar joints, traffic barrier, and asphalt paving.
Project Assumptions	Locations include Clevelandtown Road Structure 06/01 (ADC Map 11, A-6), Netz Road Structure 06/03 (ADC Map 32, F-7), Lehmans Mill Road Structure 09/01 (ADC Map 11, A-6), and Beaver Creek Church Road Structure 16/02 (ADC Map 27, E-2).
Project Justification	This project is needed to extend the useful life of the structures in a cost-effective manner and to preserve the structures in a serviceable condition.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	200,000	0	0	0	0	0	0	0	200,000
Inspection	55,000	0	0	0	0	0	0	0	55,000
Land Acquisition	15,000	0	0	0	0	0	0	0	15,000
Total Cost	270,000	0	0	0	0	0	0	0	270,000
<b>Funding Sources:</b>									
General Fund	270,000	0	0	0	0	0	0	0	270,000
Total Funding	270,000	0	0	0	0	0	0	0	270,000



Project Title	Willow Road Culvert 05/10
Full-time Employees	0
Operating Costs	\$0



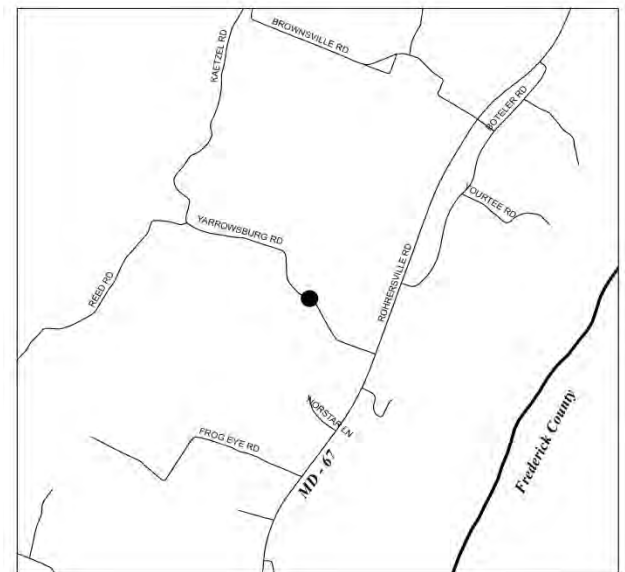
Project Description	This project is located in the 12400 block of Willow Road (ADC Map 15, Grid F-4). The project will replace the concrete slab with a concrete box culvert with headwalls.
Project Assumptions	The bridge is in a condition beyond serviceable repair.
Project Justification	This bridge is structurally and functionally deficient and requires replacement.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	246,000	0	0	0	0	0	0	0	246,000
Inspection	42,000	0	0	0	0	0	0	0	42,000
Land Acquisition	12,000	0	0	0	0	0	0	0	12,000
Utilities	23,000	0	0	0	0	0	0	0	23,000
Total Cost	323,000	0	0	0	0	0	0	0	323,000
<b>Funding Sources:</b>									
Tax-Supported Bond	323,000	0	0	0	0	0	0	0	323,000
Total Funding	323,000	0	0	0	0	0	0	0	323,000

**Project Title** Yarrowsburg Road Bridge W6191

**Full-time Employees** 0

**Operating Costs** \$0



**Project Description** This project is located just south of Brownsville Road on Yarrowsburg Road (ADC Map 37, Grid E-2). The project will replace the existing concrete encased steel beam bridge with a single span concrete bridge. The project includes a roadway approach realignment and improvements.

**Project Assumptions** The project is proposed as a Federal Aid project with 80/20 cost share for construction.

**Project Justification** The project is needed to replace a structure that is nearing the end of its useful life and to improve safety. Due to the structure type, no practical long-term repair option exists. The Bridge Sufficiency Rating is 42.1 out of a maximum possible score of 100.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Construction	1,260,000	0	0	0	0	0	0	0	1,260,000
Engineering and Design	620,000	0	0	0	0	0	0	0	620,000
Inspection	160,000	0	0	0	0	0	0	0	160,000
Land Acquisition	31,000	0	0	0	0	0	0	0	31,000
Utilities	31,000	0	0	0	0	0	0	0	31,000
<b>Total Cost</b>	<b>2,102,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>2,102,000</b>
<b><u>Funding Sources:</u></b>									
Tax-Supported Bond	470,000	0	0	0	0	0	0	0	470,000
Federal Grant	1,632,000	0	0	0	0	0	0	0	1,632,000
<b>Total Funding</b>	<b>2,102,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>2,102,000</b>

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# Drainage

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*Drainage*  
*Capital Improvement Ten Year Summary*  
*Fiscal Year 2021 - 2030*

Page	Project	Total	Prior Appr.	Budget Year		Ten Year Capital Program					Future
				FY 2021	FY 2022	FY 2023	FY 2024	FY 2025	FY 2026		
Project Costs											
	<u>Drainage</u>										
88	Stream Restoration at Various Locations	2,080,782	594,782	250,000	100,000	0	381,000	0	0	755,000	
89	Stormwater Retrofits	12,413,099	3,314,099	336,000	900,000	900,000	962,000	979,000	996,000	4,026,000	
90	Drainage Improvements at Various Locations	825,000	75,000	75,000	75,000	75,000	75,000	75,000	75,000	300,000	
91	Broadfording Church Road Culvert	231,000	0	0	0	0	57,000	174,000	0	0	
92	Draper Road Drainage Improvements	509,000	0	0	0	0	0	0	0	509,000	
93	Harpers Ferry Road Drainage, 3600 Block	376,000	0	0	0	0	75,000	301,000	0	0	
94	Shank Road Drainage	153,000	0	0	0	0	0	153,000	0	0	
95	University Road Culvert	203,000	0	0	0	0	0	0	203,000	0	
	<b>Drainage Total</b>	<b>16,790,881</b>	<b>3,983,881</b>	<b>661,000</b>	<b>1,075,000</b>	<b>975,000</b>	<b>1,550,000</b>	<b>1,682,000</b>	<b>1,274,000</b>	<b>5,590,000</b>	
Funding Sources											
	General Fund	8,435,644	1,947,644	0	385,000	837,000	688,000	613,000	671,000	3,294,000	
	Tax-Supported Bond	4,687,237	1,882,237	0	0	0	132,000	628,000	203,000	1,842,000	
	Transfer Tax	2,933,000	0	80,000	690,000	138,000	730,000	441,000	400,000	454,000	
	Capital Reserve - General	485,000	154,000	331,000	0	0	0	0	0	0	
	State Grant	250,000	0	250,000	0	0	0	0	0	0	
		<b>16,790,881</b>	<b>3,983,881</b>	<b>661,000</b>	<b>1,075,000</b>	<b>975,000</b>	<b>1,550,000</b>	<b>1,682,000</b>	<b>1,274,000</b>	<b>5,590,000</b>	

Project Title

Stream Restoration at Various Locations

Project ID

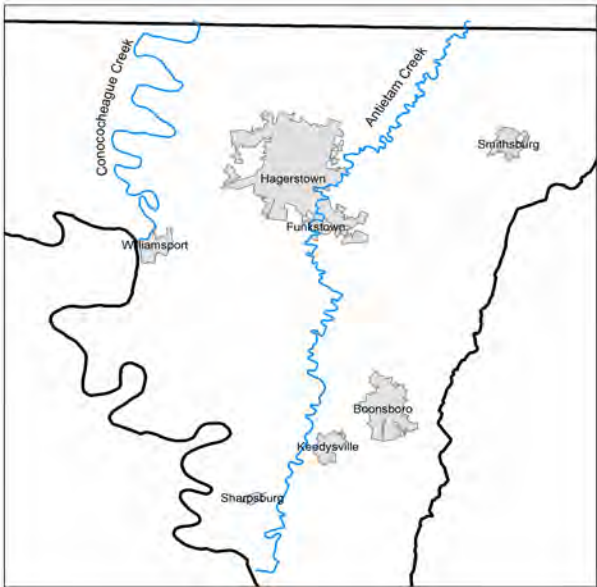
DNG030

Full-time Employees

0

Operating Costs

\$0



Project Description

This project will restore stream banks at various locations on Antietam Creek and Conococheague Creek to improve water quality.

Project Assumptions

Stream restoration projects for FY21, FY24, and FY27 are yet to be determined.

Project Justification

Maryland has imposed total maximum daily load requirements (TMDL) on Antietam Creek and Conococheague Creek. This requirement will restrict land development over time unless dealt with by mitigation measures. Stream restoration will improve water quality and allow for future development.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	2,080,782	594,782	250,000	100,000	0	381,000	0	0	755,000
<b>Total Cost</b>	<b>2,080,782</b>	<b>594,782</b>	<b>250,000</b>	<b>100,000</b>	<b>0</b>	<b>381,000</b>	<b>0</b>	<b>0</b>	<b>755,000</b>
<b>Funding Sources:</b>									
General Fund	1,830,782	594,782	0	100,000	0	381,000	0	0	755,000
State Grant	250,000	0	250,000	0	0	0	0	0	0
<b>Total Funding</b>	<b>2,080,782</b>	<b>594,782</b>	<b>250,000</b>	<b>100,000</b>	<b>0</b>	<b>381,000</b>	<b>0</b>	<b>0</b>	<b>755,000</b>

Project Title	Stormwater Retrofits
Project ID	DNG039
Full-time Employees	0
Operating Costs	\$0

Project Description	This project includes the construction of SWM systems based on Best Management Practices such as bio-swales, bio-filters, permeable pavements, ponds, wetlands, etc. along roadways, in parks, and on other county properties to satisfy the National Pollutant Discharge Elimination Systems (NPDES) requirements established by the MDE.
Project Assumptions	The project will provide treatment for 20% of the impervious surfaces within the regulated NPDES area estimated to be 3,500 acres, treating 700 acres. County is working with Army Corps of Engineers (ACOE) to inventory county stormwater infrastructure.
Project Justification	The project is requested due to a regulatory requirement imposed by the EPA and MDE.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	11,750,616	3,155,616	288,000	852,000	851,000	912,000	928,000	944,000	3,820,000
Engineering and Design	366,483	96,483	26,000	26,000	27,000	27,000	28,000	28,000	108,000
Land Acquisition	296,000	62,000	22,000	22,000	22,000	23,000	23,000	24,000	98,000
Total Cost	12,413,099	3,314,099	336,000	900,000	900,000	962,000	979,000	996,000	4,026,000
<b>Funding Sources:</b>									
General Fund	5,929,862	1,352,862	0	210,000	762,000	232,000	538,000	596,000	2,239,000
Tax-Supported Bond	3,215,237	1,882,237	0	0	0	0	0	0	1,333,000
Transfer Tax	2,933,000	0	80,000	690,000	138,000	730,000	441,000	400,000	454,000
Capital Reserve - General	335,000	79,000	256,000	0	0	0	0	0	0
Total Funding	12,413,099	3,314,099	336,000	900,000	900,000	962,000	979,000	996,000	4,026,000

Project Title	Drainage Improvements at Various Locations
Project ID	DNG080
Full-time Employees	0
Operating Costs	\$0

Project Description	The project includes providing drainage repairs and improvements for smaller scale drainage issues. These projects typically involve coordination with the Highway Department.
Project Assumptions	It is anticipated the projects will mostly involve repairs and improvements to structures not on the County inventory (less than 6' span).
Project Justification	These projects are needed to correct drainage issues in County right of way.

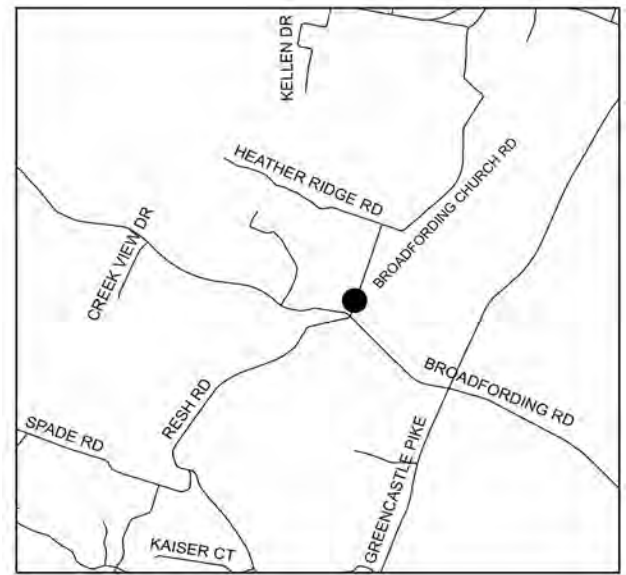
	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	755,000	65,000	65,000	65,000	75,000	65,000	75,000	65,000	280,000
Inspection	35,000	5,000	5,000	5,000	0	5,000	0	5,000	10,000
Land Acquisition	35,000	5,000	5,000	5,000	0	5,000	0	5,000	10,000
<b>Total Cost</b>	<b>825,000</b>	<b>75,000</b>	<b>75,000</b>	<b>75,000</b>	<b>75,000</b>	<b>75,000</b>	<b>75,000</b>	<b>75,000</b>	<b>300,000</b>
<b>Funding Sources:</b>									
General Fund	675,000	0	0	75,000	75,000	75,000	75,000	75,000	300,000
Capital Reserve - General	150,000	75,000	75,000	0	0	0	0	0	0
<b>Total Funding</b>	<b>825,000</b>	<b>75,000</b>	<b>75,000</b>	<b>75,000</b>	<b>75,000</b>	<b>75,000</b>	<b>75,000</b>	<b>75,000</b>	<b>300,000</b>



**Project Title**                      **Broadfording Church Road Culvert**

**Full-time Employees**            0

**Operating Costs**                  \$0



**Project Description**      This project is located in the 13500 block of Broadfording Church Road (ADC Map 9, Grid C-10). The project will replace a stone/concrete drainage structure and endwalls with a concrete pipe with concrete headwalls. The traffic barrier will be upgraded to improve safety.

**Project Justification**      The project is needed to replace a structure that is near the end of its useful life and improve vehicle safety.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
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**Project Costs:**

Construction	192,000	0	0	0	0	49,000	143,000	0	0
Inspection	31,000	0	0	0	0	0	31,000	0	0
Land Acquisition	5,000	0	0	0	0	5,000	0	0	0
Utilities	3,000	0	0	0	0	3,000	0	0	0
<b>Total Cost</b>	<b>231,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>57,000</b>	<b>174,000</b>	<b>0</b>	<b>0</b>

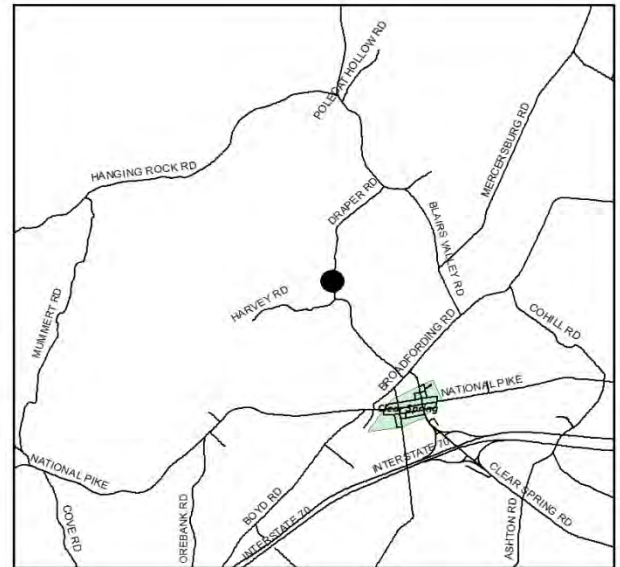
**Funding Sources:**

Tax-Supported Bond	231,000	0	0	0	0	57,000	174,000	0	0
<b>Total Funding</b>	<b>231,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>57,000</b>	<b>174,000</b>	<b>0</b>	<b>0</b>

**Project Title** Draper Road Drainage Improvements

**Full-time Employees** 0

**Operating Costs** \$0



**Project Description** This project is located in the 13000 block of Draper Road (ADC Map 7, B-11). The project will replace existing drainage pipes and install a larger storm drain network that will correct road and property flooding problems.

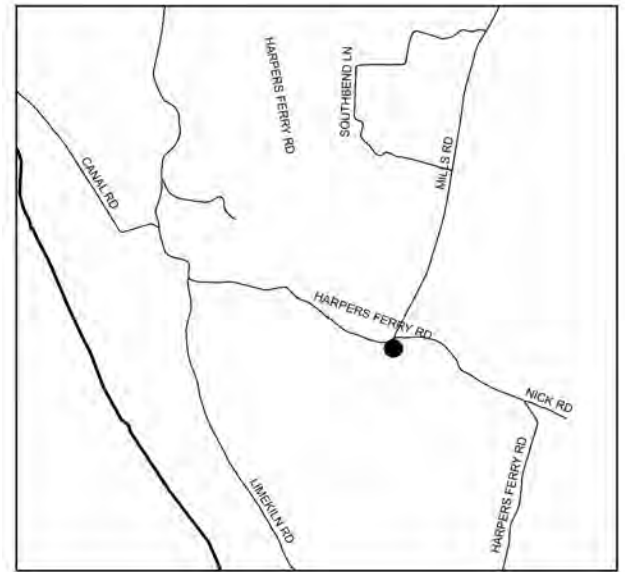
**Project Justification** This project is needed to replace existing undersized and disconnected storm drainpipes with a larger storm drain and swale network to reduce flooding problems and road maintenance.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Construction	425,000	0	0	0	0	0	0	0	425,000
Inspection	44,000	0	0	0	0	0	0	0	44,000
Land Acquisition	20,000	0	0	0	0	0	0	0	20,000
Utilities	20,000	0	0	0	0	0	0	0	20,000
<b>Total Cost</b>	<b>509,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>509,000</b>
<b><u>Funding Sources:</u></b>									
Tax-Supported Bond	509,000	0	0	0	0	0	0	0	509,000
<b>Total Funding</b>	<b>509,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>509,000</b>

**Project Title**                      **Harpers Ferry Road Drainage, 3600 Block**

**Full-time Employees**            0

**Operating Costs**                 \$0



**Project Description**            This project is located in the 3600 block of Harpers Ferry Road at the intersection with Mills Road (ADC Map 34, C-7). The project will replace the existing corrugated metal pipes with larger concrete pipes to reduce flooding impacts in this area.

**Project Justification**            The project is needed to provide cost effective road maintenance in this area. The frequency of flooding impacts the delivery of public safety services in that area of the county.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	276,000	0	0	0	0	65,000	211,000	0	0
Inspection	38,000	0	0	0	0	0	38,000	0	0
Land Acquisition	10,000	0	0	0	0	10,000	0	0	0
Utilities	52,000	0	0	0	0	0	52,000	0	0
<b>Total Cost</b>	<b>376,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>75,000</b>	<b>301,000</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
Tax-Supported Bond	376,000	0	0	0	0	75,000	301,000	0	0
<b>Total Funding</b>	<b>376,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>75,000</b>	<b>301,000</b>	<b>0</b>	<b>0</b>

Project Title

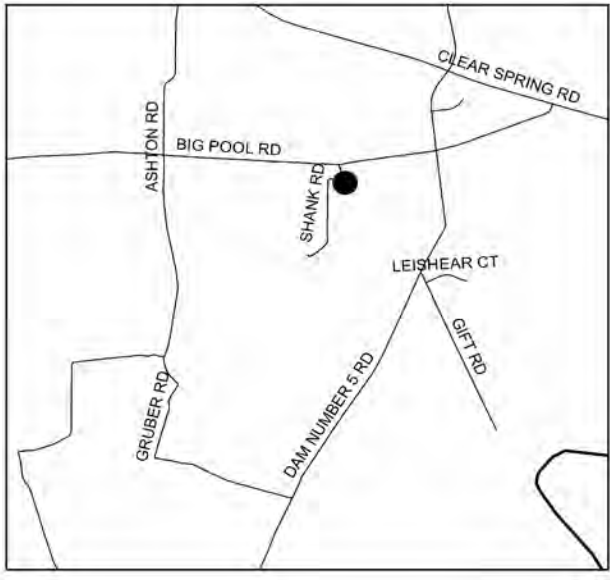
Shank Road Drainage

Full-time Employees

0

Operating Costs

\$0



Project Description

The project is located near Big Pool Road in Clear Spring (ADC Map 18, G-7). The project will stabilize the stream bank and roadway embankment.

Project Justification

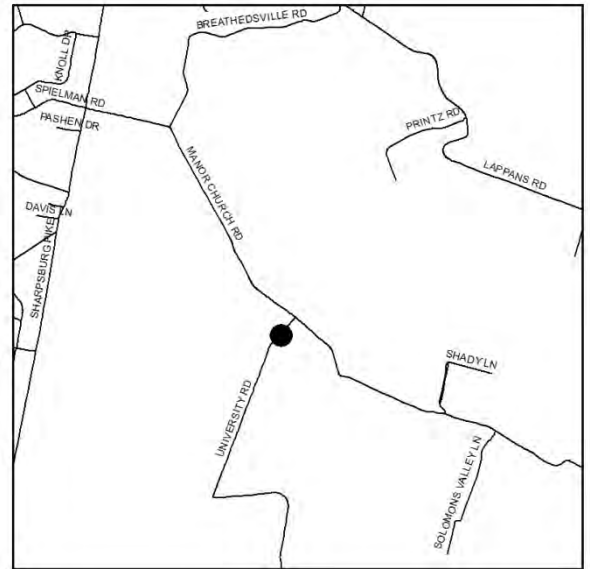
The existing retaining wall is collapsing into the adjacent stream. Failure of the wall will cause properties to become inaccessible for emergency services and will cause more costly damage.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	130,000	0	0	0	0	0	130,000	0	0
Inspection	21,000	0	0	0	0	0	21,000	0	0
Land Acquisition	2,000	0	0	0	0	0	2,000	0	0
<b>Total Cost</b>	<b>153,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>153,000</b>	<b>0</b>	<b>0</b>
<b>Funding Sources:</b>									
Tax-Supported Bond	153,000	0	0	0	0	0	153,000	0	0
<b>Total Funding</b>	<b>153,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>153,000</b>	<b>0</b>	<b>0</b>

**Project Title**                      **University Road Culvert**

**Full-time Employees**            0

**Operating Costs**                  \$0



**Project Description**            This project is located in the 7600 block of University Road (ADC Map 26, D-13). The project will replace a metal drainage structure with a concrete box culvert with headwalls. Traffic barrier will be installed to improve safety.

**Project Justification**            The project is needed to replace a structure that is near the end of its useful life.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	177,000	0	0	0	0	0	0	177,000	0
Inspection	21,000	0	0	0	0	0	0	21,000	0
Land Acquisition	5,000	0	0	0	0	0	0	5,000	0
<b>Total Cost</b>	<b>203,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>203,000</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
Tax-Supported Bond	203,000	0	0	0	0	0	0	203,000	0
<b>Total Funding</b>	<b>203,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>203,000</b>	<b>0</b>

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# Education

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*Education*  
*Capital Improvement Ten Year Summary*  
*Fiscal Year 2021 – 2030*

Page	Project	Total	Prior Appr.	Budget Year		Ten Year Capital Program				
				FY 2021	FY 2022	FY 2023	FY 2024	FY 2025	FY 2026	Future
Project Costs										
Education										
Board of Education										
100	Capital Maintenance - BOE	122,315,488	3,892,488	6,701,000	17,776,000	8,400,000	13,580,000	11,676,000	11,368,000	48,922,000
101	Sharpsburg Elementary School Replacement	26,728,000	20,537,000	6,191,000	0	0	0	0	0	0
102	Elementary School 1	43,806,000	0	500,000	2,000,000	19,806,000	19,500,000	2,000,000	0	0
103	Elementary School 2	43,806,000	0	0	0	500,000	2,000,000	19,768,000	19,400,000	2,138,000
104	Elementary School 3	43,806,000	0	0	0	0	0	500,000	2,000,000	41,306,000
105	Elementary School 4	42,000,000	0	0	0	0	0	0	0	42,000,000
106	Jonathan Hager Elementary School Addition	12,219,000	0	0	0	0	0	0	0	12,219,000
107	Urban Education Campus-BOE Component	19,400,000	16,848,000	2,552,000	0	0	0	0	0	0
Board of Education Total		354,080,488	41,277,488	15,944,000	19,776,000	28,706,000	35,080,000	33,944,000	32,768,000	146,585,000
Hagerstown Community College										
110	Center for Business and Entrepreneurial Studies	11,230,000	6,281,000	4,449,000	500,000	0	0	0	0	0
111	ARCC Air Conditioning	1,100,000	0	1,100,000	0	0	0	0	0	0
112	ATC Operations Building	5,400,000	0	0	0	0	0	958,000	1,690,000	2,752,000
113	Campus Road & Parking Lot Overlays	3,780,000	0	0	0	400,000	0	0	0	3,380,000
114	Career Programs Roof Replacement	1,562,000	0	0	0	0	0	0	0	1,562,000
115	CVT Instructional Facility Acquisition	2,000,000	1,400,000	0	0	600,000	0	0	0	0
116	CVT Instruction Facility Renovation/Construction Project	2,500,000	0	0	0	0	1,759,000	741,000	0	0
117	LRC Exterior Metal Panel System and Roof Replacement	1,746,000	0	0	1,746,000	0	0	0	0	0
118	Second Entrance Drive Widening Project	2,000,000	0	0	0	0	0	0	0	2,000,000
Hagerstown Community College Total		31,318,000	7,681,000	5,549,000	2,246,000	1,000,000	1,759,000	1,699,000	1,690,000	9,694,000
Public Libraries										
120	Systemic Projects - Library	131,492	41,492	0	10,000	10,000	10,000	10,000	10,000	40,000
121	Hancock Public Library Replacement	2,825,000	132,000	10,000	2,683,000	0	0	0	0	0
Public Libraries Total		2,956,492	173,492	10,000	2,693,000	10,000	10,000	10,000	10,000	40,000
Education Total		388,354,980	49,131,980	21,503,000	24,715,000	29,716,000	36,849,000	35,653,000	34,468,000	156,319,000
Funding Sources										
General Fund		1,153,250	1,143,250	0	10,000	0	0	0	0	0
Tax-Supported Bond		52,310,830	8,253,830	5,524,000	4,723,000	4,215,000	4,215,000	4,215,000	4,215,000	16,950,000
Transfer Tax		848,000	0	0	848,000	0	0	0	0	0
Excise Tax - Schools		5,005,000	1,155,000	385,000	385,000	385,000	385,000	385,000	385,000	1,540,000
Excise Tax - Library		163,900	63,900	10,000	10,000	10,000	10,000	10,000	10,000	40,000
State Grant		240,266,000	29,970,000	7,748,000	15,086,000	19,325,000	23,738,000	23,410,000	22,109,000	98,880,000
Contributions		88,608,000	8,546,000	7,836,000	3,653,000	5,781,000	8,501,000	7,633,000	7,749,000	38,909,000
		388,354,980	49,131,980	21,503,000	24,715,000	29,716,000	36,849,000	35,653,000	34,468,000	156,319,000



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*Board of Education*  
*Capital Improvement Ten Year Summary*  
*Fiscal Year 2021 - 2030*

		Budget Year				Ten Year Capital Program				
Page	Project	Total	Prior Appr.	FY 2021	FY 2022	FY 2023	FY 2024	FY 2025	FY 2026	Future
Project Costs										
Board of Education										
100	Capital Maintenance - BOE	122,315,488	3,892,488	6,701,000	17,776,000	8,400,000	13,580,000	11,676,000	11,368,000	48,922,000
101	Sharpsburg Elementary School Replacement	26,728,000	20,537,000	6,191,000	0	0	0	0	0	0
102	Elementary School 1	43,806,000	0	500,000	2,000,000	19,806,000	19,500,000	2,000,000	0	0
103	Elementary School 2	43,806,000	0	0	0	500,000	2,000,000	19,768,000	19,400,000	2,138,000
104	Elementary School 3	43,806,000	0	0	0	0	0	500,000	2,000,000	41,306,000
105	Elementary School 4	42,000,000	0	0	0	0	0	0	0	42,000,000
106	Jonathan Hager Elementary School Addition	12,219,000	0	0	0	0	0	0	0	12,219,000
107	Urban Education Campus-BOE Component	19,400,000	16,848,000	2,552,000	0	0	0	0	0	0
Board of Education Total		354,080,488	41,277,488	15,944,000	19,776,000	28,706,000	35,080,000	33,944,000	32,768,000	146,585,000
Funding Sources										
General Fund		1,033,658	1,033,658	0	0	0	0	0	0	0
Tax Supported Bond		44,303,830	8,153,830	3,615,000	3,615,000	3,615,000	3,615,000	3,615,000	3,615,000	14,460,000
Excise Tax - Schools		5,005,000	1,155,000	385,000	385,000	385,000	385,000	385,000	385,000	1,540,000
State Grant		223,078,000	25,862,000	6,935,000	12,663,000	18,925,000	22,579,000	22,311,000	21,019,000	92,784,000
Contributions		80,660,000	5,073,000	5,009,000	3,113,000	5,781,000	8,501,000	7,633,000	7,749,000	37,801,000
		354,080,488	41,277,488	15,944,000	19,776,000	28,706,000	35,080,000	33,944,000	32,768,000	146,585,000

Project Title

Capital Maintenance - BOE

Project ID

SCH006

Full-time Employees

0

Operating Costs

\$0

Project Description

Projects vary depending on the conditions, safety, security, and utility requirements. The Comprehensive Maintenance Plan outlines specific projects over the next five years. Projects which qualify for State funding of 79% of construction costs are included here. Projects are targeted to reduce deferred maintenance.

Project Assumptions

FY2021 - Funding is needed for roof replacements at South Hagerstown High School and Western Heights Middle School, an HVAC replacement at Smithsburg High School, and a chiller replacement at North Hagerstown High School.

Project Justification

The projects consist of large dollar, long-lived systemic projects that qualify for State funding outside the operating budget.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Architect Fees	554,000	100,000	454,000	0	0	0	0	0	0
Construction	120,909,488	2,940,488	6,247,000	17,776,000	8,400,000	13,580,000	11,676,000	11,368,000	48,922,000
Equipment/Furniture	852,000	852,000	0	0	0	0	0	0	0
Total Cost	122,315,488	3,892,488	6,701,000	17,776,000	8,400,000	13,580,000	11,676,000	11,368,000	48,922,000
<b>Funding Sources:</b>									
General Fund	1,033,658	1,033,658	0	0	0	0	0	0	0
Tax-Supported Bond	15,869,830	1,793,830	76,000	2,000,000	1,500,000	1,500,000	1,500,000	1,500,000	6,000,000
State Grant	79,851,000	1,065,000	4,405,000	12,663,000	5,925,000	9,579,000	7,742,000	8,019,000	30,453,000
Contributions	25,561,000	0	2,220,000	3,113,000	975,000	2,501,000	2,434,000	1,849,000	12,469,000
Total Funding	122,315,488	3,892,488	6,701,000	17,776,000	8,400,000	13,580,000	11,676,000	11,368,000	48,922,000

Project Title

Sharpsburg Elementary School- Replacement

Project ID

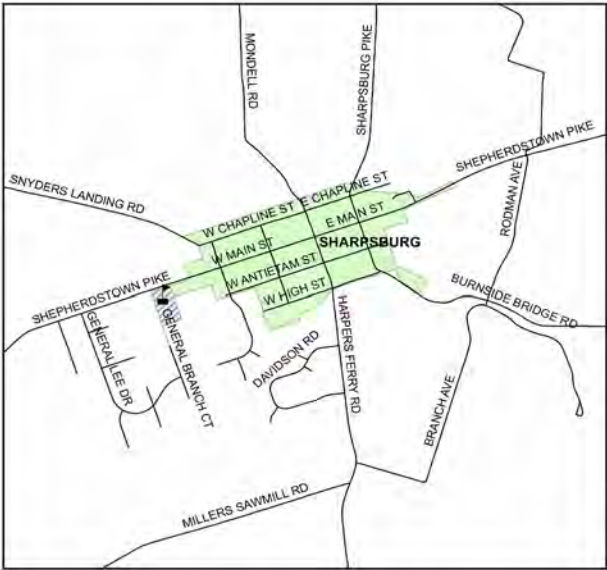
SCH033

Full-time Employees

0

Operating Costs

\$0



Project Description

The project involves construction of 60,054 SF replacement building to support 473 students.

Project Assumptions

State share is 71% of eligible construction costs.

Project Justification

Replacement is needed due to the age of the school and growth in the southern part of the county.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<strong>Project Costs:</strong>									
Architect Fees	1,785,000	1,735,000	50,000	0	0	0	0	0	0
Construction	23,783,000	18,252,000	5,531,000	0	0	0	0	0	0
Equipment/Furniture	1,160,000	550,000	610,000	0	0	0	0	0	0
Total Cost	26,728,000	20,537,000	6,191,000	0	0	0	0	0	0
<strong>Funding Sources:</strong>									
Tax-Supported Bond	9,899,000	6,360,000	3,539,000	0	0	0	0	0	0
Excise Tax - Schools	1,540,000	1,155,000	385,000	0	0	0	0	0	0
State Grant	15,289,000	13,022,000	2,267,000	0	0	0	0	0	0
Total Funding	26,728,000	20,537,000	6,191,000	0	0	0	0	0	0

Project Title	Elementary School 1 (Hickory and Fountain Rock Closure)
Full-time Employees	0
Operating Costs	\$(1,249,000)

Project Description	New 5-round facility to provide capacity to close 2 aging school facilities.
Project Assumptions	To be built on land already owned at CES.
Project Justification	Old schools are "open concept" and require use of portable classroom. Fountain Rock is not served by public water and sewer systems. Community use spaces will be provided.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Architect Fees	2,552,000	0	500,000	1,800,000	100,000	100,000	52,000	0	0
Construction	39,111,000	0	0	200,000	19,706,000	18,300,000	905,000	0	0
Equipment/Furniture	2,143,000	0	0	0	0	1,100,000	1,043,000	0	0
Total Cost	43,806,000	0	500,000	2,000,000	19,806,000	19,500,000	2,000,000	0	0
<b>Funding Sources:</b>									
Tax-Supported Bond	5,891,000	0	0	1,615,000	2,115,000	2,115,000	46,000	0	0
Excise Tax - Schools	1,540,000	0	0	385,000	385,000	385,000	385,000	0	0
State Grant	27,569,000	0	0	0	13,000,000	13,000,000	1,569,000	0	0
Contributions	8,806,000	0	500,000	0	4,306,000	4,000,000	0	0	0
Total Funding	43,806,000	0	500,000	2,000,000	19,806,000	19,500,000	2,000,000	0	0

Project Title	Elementary School 2 (Greenbrier, Old Forge, and Cascade Closure)
Full-time Employees	0
Operating Costs	\$(1,249,000)

Project Description	New 5-round facility to provide capacity to allow closing of 3 aging school facilities.
Project Assumptions	An appropriate site for this school will have to be acquired. Costs do not include land.
Project Justification	The older schools are "open concept" schools and require the use of 5 portable classrooms. Two of them are not served by public water and sewer systems. Community use spaces will be included.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Architect Fees	2,552,000	0	0	0	500,000	1,800,000	100,000	100,000	52,000
Construction	39,111,000	0	0	0	0	200,000	19,668,000	18,200,000	1,043,000
Equipment/Furniture	2,143,000	0	0	0	0	0	0	1,100,000	1,043,000
Total Cost	43,806,000	0	0	0	500,000	2,000,000	19,768,000	19,400,000	2,138,000
<b>Funding Sources:</b>									
Tax-Supported Bond	4,247,000	0	0	0	0	0	2,069,000	2,115,000	63,000
Excise Tax - Schools	385,000	0	0	0	0	0	0	385,000	0
State Grant	28,075,000	0	0	0	0	0	13,000,000	13,000,000	2,075,000
Contributions	11,099,000	0	0	0	500,000	2,000,000	4,699,000	3,900,000	0
Total Funding	43,806,000	0	0	0	500,000	2,000,000	19,768,000	19,400,000	2,138,000

Project Title	Elementary School 3 (Fountaindale and Potomac Heights Closure)
Full-time Employees	0
Operating Costs	\$(1,249,000)

Project Description	New 5-round facility to provide capacity to allow closing of 2 aging school facilities.
Project Assumptions	An appropriate site will have to be acquired. Costs do not include land.
Project Justification	Potomac Heights is an "open concept" school and the 2 schools require 6 portable classrooms. Community use spaces will be included.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
Project Costs:									
Architect Fees	2,552,000	0	0	0	0	0	500,000	1,800,000	252,000
Construction	39,111,000	0	0	0	0	0	0	200,000	38,911,000
Equipment/Furniture	2,143,000	0	0	0	0	0	0	0	2,143,000
Total Cost	43,806,000	0	0	0	0	0	500,000	2,000,000	41,306,000
Funding Sources:									
Tax-Supported Bond	4,804,000	0	0	0	0	0	0	0	4,804,000
Excise Tax - Schools	1,155,000	0	0	0	0	0	0	0	1,155,000
State Grant	26,284,000	0	0	0	0	0	0	0	26,284,000
Contributions	11,563,000	0	0	0	0	0	500,000	2,000,000	9,063,000
Total Funding	43,806,000	0	0	0	0	0	500,000	2,000,000	41,306,000

Project Title	Elementary School 4 (Emma K. Doub and Funkstown Closure)
Full-time Employees	0
Operating Costs	\$(1,249,000)

Project Description	New 5-round facility to provide capacity to allow closing of 2 aging school facilities.
Project Assumptions	New school can be built on existing Emma K. Doub site.
Project Justification	Both schools are semi-open concept schools and the 2 schools require 6 portable classrooms. Community use spaces will be included.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Architect Fees	2,600,000	0	0	0	0	0	0	0	2,600,000
Construction	38,300,000	0	0	0	0	0	0	0	38,300,000
Equipment/Furniture	1,100,000	0	0	0	0	0	0	0	1,100,000
<b>Total Cost</b>	<b>42,000,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>42,000,000</b>
<b>Funding Sources:</b>									
Tax-Supported Bond	3,593,000	0	0	0	0	0	0	0	3,593,000
Excise Tax - Schools	385,000	0	0	0	0	0	0	0	385,000
State Grant	26,000,000	0	0	0	0	0	0	0	26,000,000
Contributions	12,022,000	0	0	0	0	0	0	0	12,022,000
<b>Total Funding</b>	<b>42,000,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>42,000,000</b>



Project Title

Jonathan Hager Elementary School Addition

Full-time Employees

0

Operating Costs

\$0



Project Description

Provide additional 314 seat capacity to a maximum of 785 students.

Project Justification

Continued planned development in the West area of Hagerstown and vicinity.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Architect Fees	713,000	0	0	0	0	0	0	0	713,000
Construction	10,901,000	0	0	0	0	0	0	0	10,901,000
Equipment/Furniture	605,000	0	0	0	0	0	0	0	605,000
Total Cost	12,219,000	0	0	0	0	0	0	0	12,219,000
<b>Funding Sources:</b>									
State Grant	7,972,000	0	0	0	0	0	0	0	7,972,000
Contributions	4,247,000	0	0	0	0	0	0	0	4,247,000
Total Funding	12,219,000	0	0	0	0	0	0	0	12,219,000

Project Title                      Urban Education Campus-BOE Component

Full-time Employees            0

Operating Costs                \$0

Project Description            This represents the Board's and State component of the intergovernmental partnership that supports the revitalization of the urban core of Hagerstown.

Project Assumptions           State funding is a combination of IAC funds, Governor’s grants and strategic demolition funds.

Project Justification           Commissioners response to need identified by Board and other organizations.

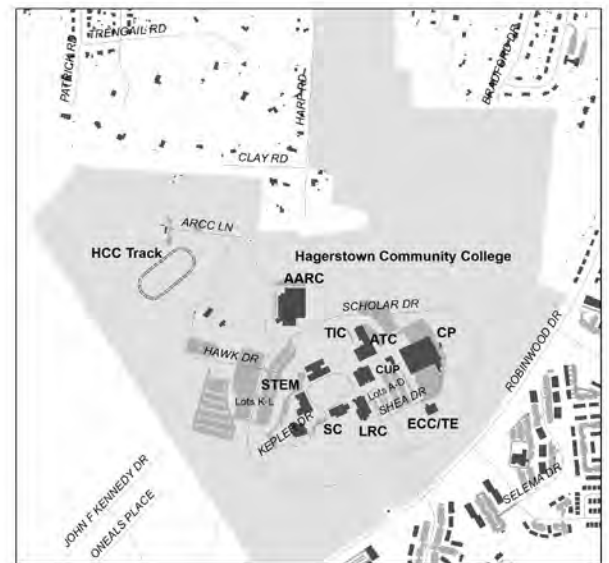
	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	19,400,000	16,848,000	2,552,000	0	0	0	0	0	0
<b>Total Cost</b>	<b>19,400,000</b>	<b>16,848,000</b>	<b>2,552,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>Funding Sources:</b>									
State Grant	12,038,000	11,775,000	263,000	0	0	0	0	0	0
Contributions	7,362,000	5,073,000	2,289,000	0	0	0	0	0	0
<b>Total Funding</b>	<b>19,400,000</b>	<b>16,848,000</b>	<b>2,552,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

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*Hagerstown Community College*  
*Capital Improvement Ten Year Summary*  
*Fiscal Year 2021 – 2030*

Page	Project	Budget Year				Ten Year Capital Program					Future
		Total	Prior Appr.	FY 2021	FY 2022	FY 2023	FY 2024	FY 2025	FY 2026		
Project Costs											
<u>Hagerstown Community College</u>											
110	Center for Business and Entrepreneurial Studies	11,230,000	6,281,000	4,449,000	500,000	0	0	0	0	0	
111	ARCC Air Conditioning	1,100,000	0	1,100,000	0	0	0	0	0	0	
112	ATC Operations Building	5,400,000	0	0	0	0	0	958,000	1,690,000	2,752,000	
113	Campus Road & Parking Lot Overlays	3,780,000	0	0	0	400,000	0	0	0	3,380,000	
114	Career Programs Roof Replacement	1,562,000	0	0	0	0	0	0	0	1,562,000	
115	CVT Instructional Facility Acquisition	2,000,000	1,400,000	0	0	600,000	0	0	0	0	
116	CVT Instructional Facility Renovation/Construction Project	2,500,000	0	0	0	0	1,759,000	741,000	0	0	
117	LRC Exterior Metal Panel System and Roof Replacement	1,746,000	0	0	1,746,000	0	0	0	0	0	
118	Second Entrance Drive Widening Project	2,000,000	0	0	0	0	0	0	0	2,000,000	
	<b>Hagerstown Community College Total</b>	<b>31,318,000</b>	<b>7,681,000</b>	<b>5,549,000</b>	<b>2,246,000</b>	<b>1,000,000</b>	<b>1,759,000</b>	<b>1,699,000</b>	<b>1,690,000</b>	<b>9,694,000</b>	
Funding Sources											
	Tax Supported Bond	8,007,000	100,000	1,909,000	1,108,000	600,000	600,000	600,000	600,000	2,490,000	
	State Grant	15,903,000	4,108,000	813,000	1,138,000	400,000	1,159,000	1,099,000	1,090,000	6,096,000	
	Contributions	7,408,000	3,473,000	2,827,000	0	0	0	0	0	1,108,000	
		<b>31,318,000</b>	<b>7,681,000</b>	<b>5,549,000</b>	<b>2,246,000</b>	<b>1,000,000</b>	<b>1,759,000</b>	<b>1,699,000</b>	<b>1,690,000</b>	<b>9,694,000</b>	

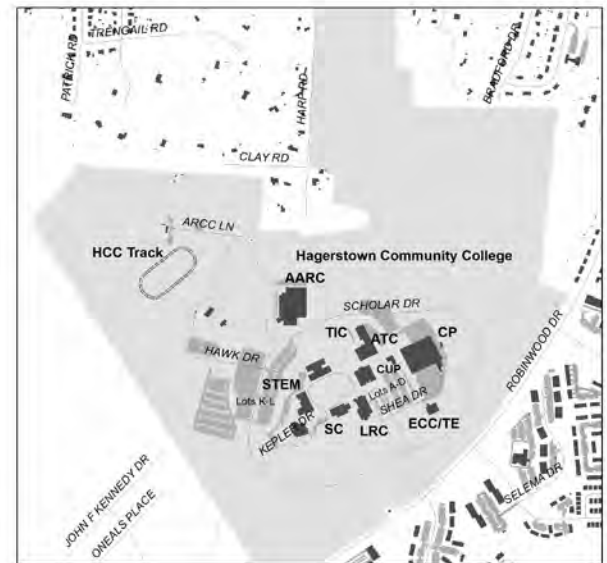
<b>Project Title</b>	<b>Center for Business and Entrepreneurial Studies</b>
<b>Project ID</b>	COL026
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$0



<b>Project Description</b>	The project includes an extensive first floor renovation along with major building upgrades to create the Center for Business and Entrepreneurial Studies. The current building has large areas of space that was constructed for training in light manufacturing disciplines. As the market has changed, so has the demand for this type of space. The project will make these areas suitable for other purposes, including the creation of a business incubator. The renovation will create a lobby area, modern lab areas, classrooms and shared spaces for business incubation areas.
<b>Project Assumptions</b>	Project assumes state funding at 56% of total project costs. County funding at 20% and HCC assumes 24% of total project cost.
<b>Project Justification</b>	This project is important to the community and businesses of Hagerstown in order to provide opportunities for local startup companies in the areas of biotechnology and cyber security.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Architect Fees	100,000	100,000	0	0	0	0	0	0	0
Construction	9,429,000	5,081,000	4,348,000	0	0	0	0	0	0
Hardware/Software	262,000	262,000	0	0	0	0	0	0	0
Equipment/Furniture	1,101,000	500,000	101,000	500,000	0	0	0	0	0
Other	338,000	338,000	0	0	0	0	0	0	0
<b>Total Cost</b>	<b>11,230,000</b>	<b>6,281,000</b>	<b>4,449,000</b>	<b>500,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
Tax-Supported Bond	2,009,000	100,000	1,409,000	500,000	0	0	0	0	0
State Grant	4,921,000	4,108,000	813,000	0	0	0	0	0	0
Contributions	4,300,000	2,073,000	2,227,000	0	0	0	0	0	0
<b>Total Funding</b>	<b>11,230,000</b>	<b>6,281,000</b>	<b>4,449,000</b>	<b>500,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

<b>Project Title</b>	<b>ARCC Air Conditioning</b>
<b>Project ID</b>	COL027
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$75,000



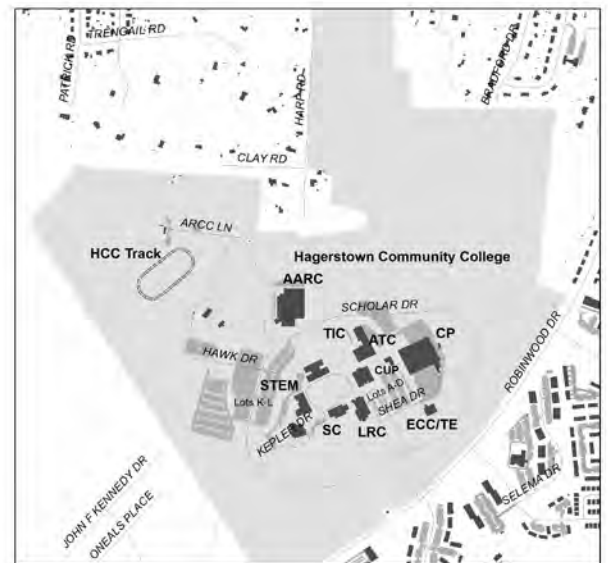
**Project Description** The ARCC (Athletic Recreation and Community Center) is the largest indoor gathering spot in Washington County, with no other facility approaching its capacity. As the largest indoor gathering spot in Washington County, the ARCC facility is in need of a space where it is climate controlled. A variety of college and community activities are held in the ARCC. These include credit and credit-free courses, educational programs, athletic events and community events. Also housed within the facility is the Washington County Recreation Department (WCRD). Approximately 50 percent of the WCRD programming takes place in the ARCC. Along with the College and WCRD activities, the ARCC also hosts community and regional events, including high school graduations, athletic competitions, home shows and other events. To increase programming for the College, WCRD, and community events, the facility needs to install air conditioning in the arena to accommodate and increase the high level of usage.

**Project Assumptions** County and College split the cost, since the County has a high utilization of the facility.

**Project Justification** Added and upgraded HVAC will improve comfort in the ARCC, making it more conducive to events in the arena. Currently, HCC is losing revenue as events are scheduled elsewhere in the summer or moved because of heat in the arena. Opposing teams have been unwilling to schedule games because of the lack of climate control. Additionally, less fluctuation in temperature and humidity will prolong the life of the hardwood floors and the indoor track, which are very expensive to replace. Improvements to the ARCC will most likely increase enrollment in physical education and leisure studies offerings, along with athletics. A more comfortable arena will help HCC recruit quality athletes for its sports teams, as well as prospective students. Continued growth and development in Washington County will increase the demand for a large venue with multiple use capability, with no solution available. Thus, HCC will miss the opportunity for partnering with the community and revenue generating opportunities.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b>Project Costs:</b>									
Architect Fees	136,000	0	136,000	0	0	0	0	0	0
Construction	964,000	0	964,000	0	0	0	0	0	0
<b>Total Cost</b>	<b>1,100,000</b>	<b>0</b>	<b>1,100,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>Funding Sources:</b>									
Tax-Supported Bond	500,000	0	500,000	0	0	0	0	0	0
Contributions	600,000	0	600,000	0	0	0	0	0	0
<b>Total Funding</b>	<b>1,100,000</b>	<b>0</b>	<b>1,100,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

<b>Project Title</b>	<b>ATC Operations Building</b>
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$2,000



**Project Description** This project combines two projects that were proposed separately in the January 2016 Facility Master Plan. The decision to merge the two projects of ATC renovation and construction of a campus operations building is being proposed for cost effectiveness and efficiency. The building has not had major renovation since 1989 and a new roof was installed in 2005. This project will support the A.A. S. in Advanced Manufacturing Systems (ADM), as well as the Industrial Technology and Electronics certificates. An addition is planned to house Maintenance, Custodial Services, Grounds, Business and Procurement Services, Mail Center and to provide a much-needed area for central receiving. High bays will be installed for that purpose, as well as to accommodate large robotic equipment for the ADM program. The addition will be built over the current ATC parking lot making construction less costly than building on undeveloped land.

**Project Assumptions** State share approximately 64.5%

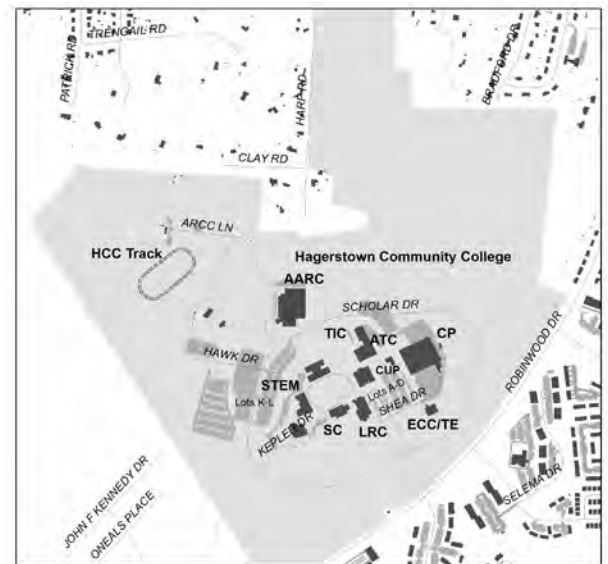
**Project Justification** Gaps related to robot programming, automation tooling and CNC machines are directly related to changing technology skills that current mechanics lack, which, in turn results in significant loss in uptime and productivity. HCC and its business partners, such as Volvo, must develop and sustain an advanced manufacturing workforce by creating pathways that span learning levels, provides support for student achievement of rigorous outcomes and ensures that students are trained using industry-standard equipment. In addition to critical workforce development, the STC, which has not had major renovations since 1989, will enhance and strengthen all STEM education programs at HCC via improved facilities and programming.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b>Project Costs:</b>									
Architect Fees	378,000	0	0	0	0	0	378,000	0	0
Construction	4,522,000	0	0	0	0	0	580,000	1,690,000	2,252,000
Equipment/Furniture	500,000	0	0	0	0	0	0	0	500,000
<b>Total Cost</b>	<b>5,400,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>958,000</b>	<b>1,690,000</b>	<b>2,752,000</b>
<b>Funding Sources:</b>									
Tax-Supported Bond	1,546,000	0	0	0	0	0	346,000	600,000	600,000
State Grant	3,527,000	0	0	0	0	0	612,000	1,090,000	1,825,000
Contributions	327,000	0	0	0	0	0	0	0	327,000
<b>Total Funding</b>	<b>5,400,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>958,000</b>	<b>1,690,000</b>	<b>2,752,000</b>

**Project Title**                      **Campus Road and Parking Lot Overlays**

**Full-time Employees**            0

**Operating Costs**                 \$0



**Project Description**    The roads and parking lots around campus are showing signs of deterioration due to high traffic, construction and snow removal. The project will consist of resurfacing of the roads around campus and parking lots. The roads included in this project are Loop Road, Hawk Drive, Quad and Kepler Drive. Some of these roads will require a fill rebuild. Parking lots in this project that need resurfaced are A, B, C, D, L and K.

**Project Assumptions**      State share approximately 61.8%.

**Project Justification**      This project is vital to the upkeep of the roads on campus as they continue to age and deteriorate. Repairing roadways and parking lots is costly and time consuming. If the project is not funded, the College may need to limit traffic on the east side of campus because the poor condition of the asphalt may jeopardize the tires, wheel alignments and undercarriages of vehicles. A new entrance is planned on the east side of campus that will add an influx of cars onto these parking lots and roads, thereby causing additional wear and damage. Student, employee and community dissatisfaction will occur as the poor condition of roads may cause vehicle damage.

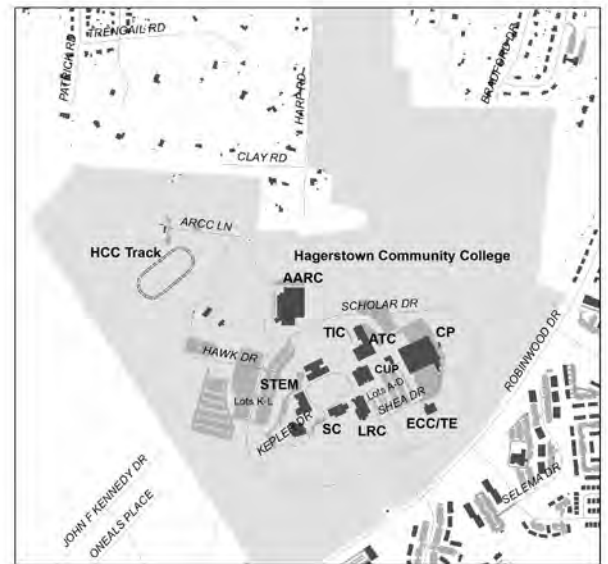
**Project Costs:**

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
Construction	3,780,000	0	0	0	400,000	0	0	0	3,380,000
<b>Total Cost</b>	<b>3,780,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>400,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>3,380,000</b>

**Funding Sources:**

Tax-Supported Bond	662,000	0	0	0	0	0	0	0	662,000
State Grant	2,337,000	0	0	0	400,000	0	0	0	1,937,000
Contributions	781,000	0	0	0	0	0	0	0	781,000
<b>Total Funding</b>	<b>3,780,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>400,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>3,380,000</b>



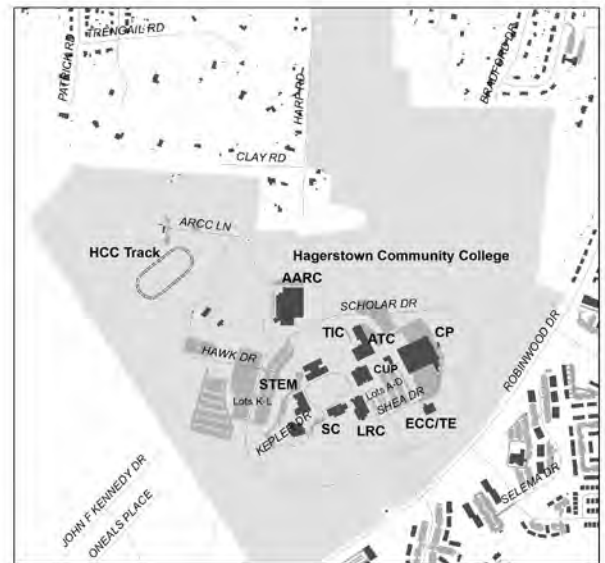
**Project Title** Career Programs Roof Replacement**Full-time Employees** 0**Operating Costs** \$0**Project Description** Replace the roof on the Career Programs Building.**Project Assumptions** This project assumes approximately 65% funding from the State and 35% funding from the County.**Project Justification** It is critical at this point in the roof's useful life to be replaced prior to water damage taking place in various parts of the building. This would create additional costs and possibly require significant renovation if this were to occur.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Construction	1,562,000	0	0	0	0	0	0	0	1,562,000
<b>Total Cost</b>	<b>1,562,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1,562,000</b>
<b><u>Funding Sources:</u></b>									
Tax-Supported Bond	538,000	0	0	0	0	0	0	0	538,000
State Grant	1,024,000	0	0	0	0	0	0	0	1,024,000
<b>Total Funding</b>	<b>1,562,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1,562,000</b>

**Project Title** CVT Instructional Facility Acquisition

**Full-time Employees** 0

**Operating Costs** \$0



**Project Description** The project calls for the purchase of 5 - 10 acres of land with an existing building of approximately 5,000 sq. ft. that can be used for the Commercial Vehicle Training, Logistics, forklift training and the drone program. The ideal project should be near the interstate. The building will need to have space for two classrooms, three offices, a dock area and a forklift training area. A parking lot and driving surfaces will be needed to handle the large trucks.

**Project Assumptions** Project assumes 18% funding from County and 82% funding from HCC.

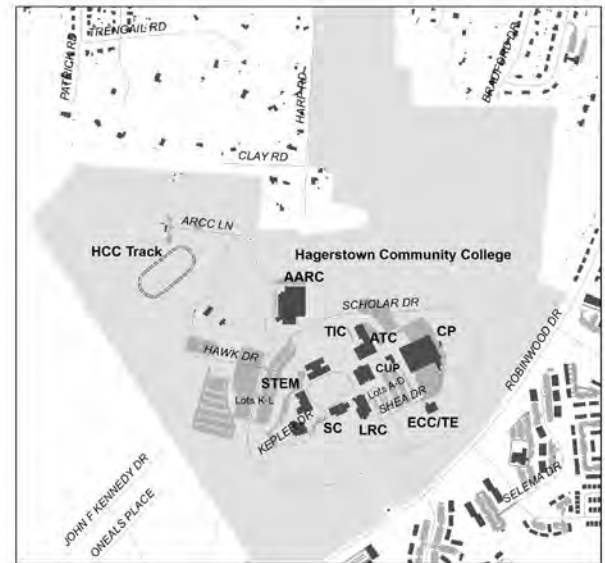
**Project Justification** This project will support programs that are critical to the local economy by providing trained and certified students ready for the transportation and warehousing industry.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Land Acquisition	2,000,000	1,400,000	0	0	600,000	0	0	0	0
<b>Total Cost</b>	<b>2,000,000</b>	<b>1,400,000</b>	<b>0</b>	<b>0</b>	<b>600,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
Tax-Supported Bond	600,000	0	0	0	600,000	0	0	0	0
Contributions	1,400,000	1,400,000	0	0	0	0	0	0	0
<b>Total Funding</b>	<b>2,000,000</b>	<b>1,400,000</b>	<b>0</b>	<b>0</b>	<b>600,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

**Project Title**                      **CVT Instruction Facility Renovation/Construction Project**

**Full-time Employees**        0

**Operating Costs**              \$25,000



**Project Description**        This building has yet to be identified, therefore the extent of renovations is not yet completely known. Once this building is acquired the facility will provide grounds for the commercial vehicle transportation program, logistics/forklift training and a drone staging area. The CVT Specialist Certificate program is a 16-credit cohort skills-oriented program for those individuals seeking a career in professional truck driving. This program is the largest retraining program for unemployed persons in Washington County. The curriculum consists of classroom, skills, and field instruction and is based on industry recognized skill standards. By providing such training, the facility will support economic development in the service region for not only the transportation industry, but for warehousing and distribution centers along the I-81 and 70 corridors.

**Project Assumptions**        Approximately 64.5% State Support

**Project Justification**        The trucking industry is a vital component to economic growth locally, statewide, and nationally, with trucks hauling 70 percent of all freight tonnage. The College's CVT program supports one of the local Economic Development Commission's workforce development priorities, which states, "The trucking industry continues to exhibit a high demand for CDL-A drivers, a national trend as well as the largest demand occupation locally." The trucking industry locally and nationally is not attracting drivers at the rate to keep up with demand and growth. According to the American Trucking Associations (ATA), the industry is about 30,000 short of qualified drivers. Over the next ten years, that number is set to rise to 200,000 in an industry that averages 115-120 percent annual turnover rate. Along with nursing and health sciences programs, the CVT program has the highest completion and placement rates at HCC. It is critical that the College have adequate facilities to train students. The College will continue to lease facilities. The current facility is located in the far north end of Hagerstown less than a mile from the Pennsylvania line. Travel times from the main campus can take up to 20-25 minutes depending on time of day and traffic, and there is no public transportation available to/from the site.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
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**Project Costs:**

Construction	1,850,000	0	0	0	0	1,609,000	241,000	0	0
Engineering and Design	150,000	0	0	0	0	150,000	0	0	0
Equipment/Furniture	500,000	0	0	0	0	0	500,000	0	0
<b>Total Cost</b>	<b>2,500,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1,759,000</b>	<b>741,000</b>	<b>0</b>	<b>0</b>

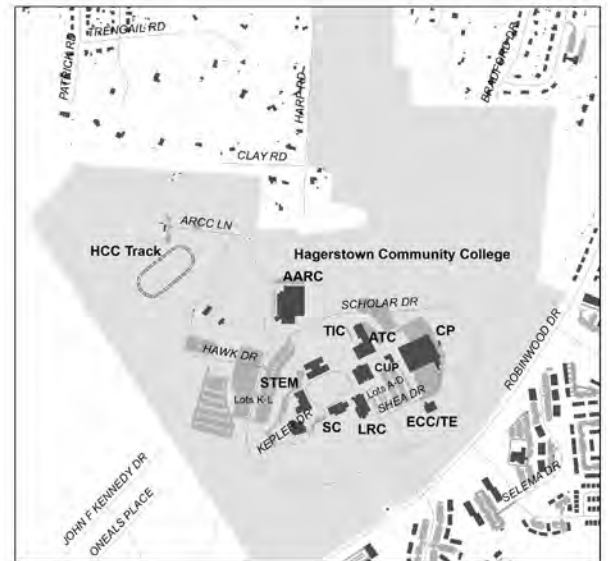
**Funding Sources:**

Tax-Supported Bond	854,000	0	0	0	0	600,000	254,000	0	0
State Grant	1,646,000	0	0	0	0	1,159,000	487,000	0	0
<b>Total Funding</b>	<b>2,500,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1,759,000</b>	<b>741,000</b>	<b>0</b>	<b>0</b>

**Project Title**                    **LRC Exterior Metal Panel System and Roof Replacement**

**Full-time Employees**        0

**Operating Costs**             \$0



**Project Description**        This project calls for the replacement of the original 22,222 SF roof, which is built-up asphalt. There are also several smaller standing seam metal roof sections. The exterior metal panel system is a first-generation system that uses a gasket and clip system to attach them to the building structure. These early generation systems have poor insulating properties and have been plagued by water infiltration issues. Many of the roof flashing details integrate with the metal panel system requiring both systems be replaced in conjunction with each other. The main roof sections will be replaced with an Energy Star rated modified bitumen system. The metal roofing sections will be replaced with new standing seam roof systems. Both roof systems will include new flashing details to prevent water infiltration. The metal panel system will be replaced with a new metal panel system that has been engineered to improve the insulating properties and performance.

**Project Assumptions**        State share approximately 65%.

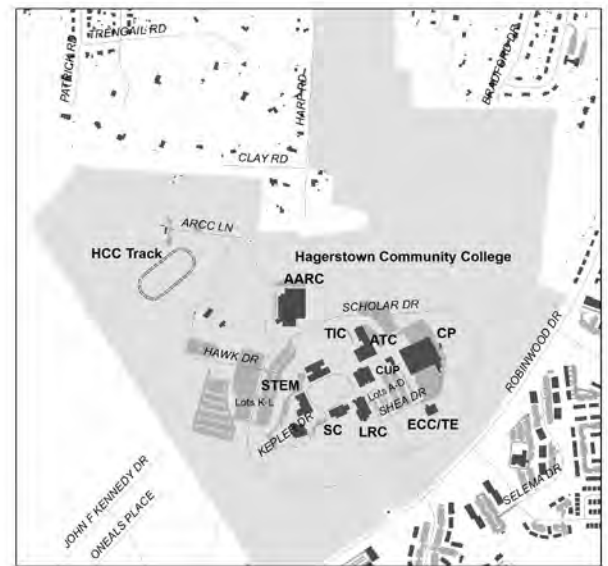
**Project Justification**        This project is needed to avoid costly repairs.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	1,591,000	0	0	1,591,000	0	0	0	0	0
Engineering and Design	155,000	0	0	155,000	0	0	0	0	0
<b>Total Cost</b>	<b>1,746,000</b>	<b>0</b>	<b>0</b>	<b>1,746,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>Funding Sources:</b>									
Tax-Supported Bond	608,000	0	0	608,000	0	0	0	0	0
State Grant	1,138,000	0	0	1,138,000	0	0	0	0	0
<b>Total Funding</b>	<b>1,746,000</b>	<b>0</b>	<b>0</b>	<b>1,746,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

**Project Title**                      **Second Entrance Drive Widening**

**Full-time Employees**            0

**Operating Costs**                 \$0



**Project Description**            The purpose of this project is to alleviate traffic and congestion and create safer traffic patterns to the secondary entrance of the college.

**Project Assumptions**          The project assumes approximately 65% funding from the State and 35% funding from the County.

**Project Justification**           This project is very important from a safety standpoint. With many large events held on campus it is imperative that the college have two proper entrances and exits in the event of an emergency. The current configuration creates an unsafe situation for just regular and moderate traffic.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	2,000,000	0	0	0	0	0	0	0	2,000,000
<b>Total Cost</b>	<b>2,000,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>2,000,000</b>
<b>Funding Sources:</b>									
Tax-Supported Bond	690,000	0	0	0	0	0	0	0	690,000
State Grant	1,310,000	0	0	0	0	0	0	0	1,310,000
<b>Total Funding</b>	<b>2,000,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>2,000,000</b>

*Public Libraries*  
*Capital Improvement Ten Year Summary*  
*Fiscal Year 2021 - 2030*

Page	Project	Total	Prior Appr.	Budget Year	Ten Year Capital Program					
				FY 2021	FY 2022	FY 2023	FY 2024	FY 2025	FY 2026	Future
Project Costs										
<u>Public Libraries</u>										
120	Systemic Projects - Library	131,492	41,492	0	10,000	10,000	10,000	10,000	10,000	40,000
121	Hancock Public Library Replacement	2,825,000	132,000	10,000	2,683,000	0	0	0	0	0
Public Libraries Total		2,956,492	173,492	10,000	2,693,000	10,000	10,000	10,000	10,000	40,000
Funding Sources										
General Fund		119,592	109,592	0	10,000	0	0	0	0	0
Transfer Tax		848,000	0	0	848,000	0	0	0	0	0
Excise Tax - Library		163,900	63,900	10,000	10,000	10,000	10,000	10,000	10,000	40,000
State Grant		1,285,000	0	0	1,285,000	0	0	0	0	0
Contributions		540,000	0	0	540,000	0	0	0	0	0
		2,956,492	173,492	10,000	2,693,000	10,000	10,000	10,000	10,000	40,000

**Project Title**                      **Systemic Projects - Library**

**Project ID**                              BLD075

**Full-time Employees**              0

**Operating Costs**                    \$0

**Project Description**              Future systemic projects could include chillers, boilers, rooftop HVAC units, cooling towers, roof replacements.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	131,492	41,492	0	10,000	10,000	10,000	10,000	10,000	40,000
<b>Total Cost</b>	<b>131,492</b>	<b>41,492</b>	<b>0</b>	<b>10,000</b>	<b>10,000</b>	<b>10,000</b>	<b>10,000</b>	<b>10,000</b>	<b>40,000</b>
<b><u>Funding Sources:</u></b>									
General Fund	51,492	41,492	0	10,000	0	0	0	0	0
Excise Tax - Library	80,000	0	0	0	10,000	10,000	10,000	10,000	40,000
<b>Total Funding</b>	<b>131,492</b>	<b>41,492</b>	<b>0</b>	<b>10,000</b>	<b>10,000</b>	<b>10,000</b>	<b>10,000</b>	<b>10,000</b>	<b>40,000</b>

<b>Project Title</b>	<b>Hancock Public Library Replacement</b>
<b>Project ID</b>	BLD077
<b>Full-time Employees</b>	2
<b>Operating Costs</b>	\$56,000



<b>Project Description</b>	This project includes the construction of a new public library in the town of Hancock. The project will include enhancements to the proposed site including parking and a stormwater management facility.
<b>Project Assumptions</b>	Library will be built with County funding not to exceed \$1 million. This represents the amount due to the Board of Trustees of the Washington County Library. The Library is forward funding the project costs with County reimbursement occurring in 2022. The estimated population served by the proposed facility is 4,358.
<b>Project Justification</b>	The existing structure is structurally and functionally deficient. The existing building experiences thermal and moisture protection problems. The building has no fire suppression system and has areas that are not in compliance with ADA requirements.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b>Project Costs:</b>									
Construction	2,615,000	0	0	2,615,000	0	0	0	0	0
Engineering and Design	210,000	132,000	10,000	68,000	0	0	0	0	0
<b>Total Cost</b>	<b>2,825,000</b>	<b>132,000</b>	<b>10,000</b>	<b>2,683,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>Funding Sources:</b>									
General Fund	68,100	68,100	0	0	0	0	0	0	0
Transfer Tax	848,000	0	0	848,000	0	0	0	0	0
Excise Tax - Library	83,900	63,900	10,000	10,000	0	0	0	0	0
State Grant	1,285,000	0	0	1,285,000	0	0	0	0	0
Contributions	540,000	0	0	540,000	0	0	0	0	0
<b>Total Funding</b>	<b>2,825,000</b>	<b>132,000</b>	<b>10,000</b>	<b>2,683,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>



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# General Government

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*General Government  
Capital Improvement Ten Year Summary  
Fiscal Year 2021 - 2030*

Page	Project	Total	Prior Appr.	Budget Year		Ten Year Capital Program					Future
				FY 2021	FY 2022	FY 2023	FY 2024	FY 2025	FY 2026		
Project Costs											
General Government											
124	Bond Issuance Costs	1,203,875	143,875	98,000	98,000	99,000	98,000	99,000	98,000	470,000	
125	Contingency - General Fund	1,846,933	1,021,933	0	75,000	75,000	75,000	100,000	100,000	400,000	
126	Systemic Improvements - Building	2,584,589	654,589	150,000	197,000	196,000	196,000	196,000	197,000	798,000	
127	Facilities Roof Repairs	1,330,000	100,000	0	130,000	100,000	100,000	150,000	275,000	475,000	
128	Information Systems Replacment Program	1,146,460	146,460	100,000	100,000	100,000	100,000	100,000	100,000	400,000	
129	Financial System Management & Upgrades	427,590	110,590	0	33,000	34,000	34,000	35,000	35,000	146,000	
130	Broadband Wireless Network Infrastructure	73,745	14,745	11,000	12,000	12,000	12,000	12,000	0	0	
131	General - Equipment and Vehicle Replacement Program	3,929,812	120,812	250,000	359,000	400,000	400,000	400,000	400,000	1,600,000	
General Government Total		12,543,004	2,313,004	609,000	1,004,000	1,016,000	1,015,000	1,092,000	1,205,000	4,289,000	
Funding Sources											
	General Fund	10,828,004	1,307,004	0	1,004,000	1,016,000	915,000	1,092,000	1,205,000	4,289,000	
	Tax-Supported Bond	100,000	100,000	0	0	0	0	0	0	0	
	Transfer Tax	100,000	0	0	0	0	100,000	0	0	0	
	Capital Reserve - General	1,515,000	906,000	609,000	0	0	0	0	0	0	
		12,543,004	2,313,004	609,000	1,004,000	1,016,000	1,015,000	1,092,000	1,205,000	4,289,000	

Project Title	Bond Issuance Costs
Project ID	ADM001
Full-time Employees	0
Operating Costs	\$0

Project Description	This project is for costs associated with the sale of bonds. The costs include fees for printing, financial advisor, bond counsel, discounts, and rating agency fees.
Project Justification	Bond issuance is required to finance the capital improvement plan.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Bond issuance	1,203,875	143,875	98,000	98,000	99,000	98,000	99,000	98,000	470,000
<b>Total Cost</b>	<b>1,203,875</b>	<b>143,875</b>	<b>98,000</b>	<b>98,000</b>	<b>99,000</b>	<b>98,000</b>	<b>99,000</b>	<b>98,000</b>	<b>470,000</b>
<b>Funding Sources:</b>									
General Fund	1,006,875	44,875	0	98,000	99,000	98,000	99,000	98,000	470,000
Capital Reserve - General	197,000	99,000	98,000	0	0	0	0	0	0
<b>Total Funding</b>	<b>1,203,875</b>	<b>143,875</b>	<b>98,000</b>	<b>98,000</b>	<b>99,000</b>	<b>98,000</b>	<b>99,000</b>	<b>98,000</b>	<b>470,000</b>

Project Title	Contingency - General Fund
Project ID	ADM002
Full-time Employees	0
Operating Costs	\$0

**Project Description** This project serves as a reserve to provide for emergency or unanticipated expenditures for all categories.

**Project Assumptions** Two potential funding mechanisms have been contemplated to provide for capital maintenance for BOE projects; 1) Energy Service Company (ESCO) or 2) Additional IAC funding. Funding of \$500K can be used for the most cost effective of the two, the one leading to the greater return on investment. This evaluation cannot be determined until proposals are received in connection with the ESCO RFP or until available IAC funding is known. Once all factors are considered, funds will be utilized accordingly.

**Project Justification** Due to the inherent uncertainty in estimating capital project costs, it is the written policy of the County to maintain project contingency. It is also good management practice to increase the Contingency due to the increase of Capital Program requests and rising number of individual projects, as one project overrun, or unanticipated project could use all available funds. In the event that the capital contingency accounts have excess funds, the annual appropriation for that purpose may be modified to reflect the source of funds as determined through the budget process.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Contingency	1,846,933	1,021,933	0	75,000	75,000	75,000	100,000	100,000	400,000
<b>Total Cost</b>	<b>1,846,933</b>	<b>1,021,933</b>	<b>0</b>	<b>75,000</b>	<b>75,000</b>	<b>75,000</b>	<b>100,000</b>	<b>100,000</b>	<b>400,000</b>
<b><u>Funding Sources:</u></b>									
General Fund	1,346,933	521,933	0	75,000	75,000	75,000	100,000	100,000	400,000
Capital Reserve - General	500,000	500,000	0	0	0	0	0	0	0
<b>Total Funding</b>	<b>1,846,933</b>	<b>1,021,933</b>	<b>0</b>	<b>75,000</b>	<b>75,000</b>	<b>75,000</b>	<b>100,000</b>	<b>100,000</b>	<b>400,000</b>

Project Title	Systemic Improvements-Buildings
Project ID	BLD078
Full-time Employees	0
Operating Costs	\$0

Project Description	This project includes a variety of upgrades and/or replacements including, but not limited to, air conditioning, weatherproofing, roofing and other building improvements.
Project Assumptions	Pending general fund monies availability.
Project Justification	Renovations are required to maintain the functionality of the buildings.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Construction	2,584,589	654,589	150,000	197,000	196,000	196,000	196,000	197,000	798,000
<b>Total Cost</b>	<b>2,584,589</b>	<b>654,589</b>	<b>150,000</b>	<b>197,000</b>	<b>196,000</b>	<b>196,000</b>	<b>196,000</b>	<b>197,000</b>	<b>798,000</b>
<b><u>Funding Sources:</u></b>									
General Fund	2,238,589	458,589	0	197,000	196,000	196,000	196,000	197,000	798,000
Capital Reserve - General	346,000	196,000	150,000	0	0	0	0	0	0
<b>Total Funding</b>	<b>2,584,589</b>	<b>654,589</b>	<b>150,000</b>	<b>197,000</b>	<b>196,000</b>	<b>196,000</b>	<b>196,000</b>	<b>197,000</b>	<b>798,000</b>

Project Title	Facilities Roof Repairs
Project ID	BLD100
Full-time Employees	0
Operating Costs	\$0

Project Description	Project includes roof repairs/replacement on County owned buildings.
Project Assumptions	Pending general fund.
Project Justification	Funding necessary to maintain integrity of County buildings.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Construction	1,330,000	100,000	0	130,000	100,000	100,000	150,000	275,000	475,000
<b>Total Cost</b>	<b>1,330,000</b>	<b>100,000</b>	<b>0</b>	<b>130,000</b>	<b>100,000</b>	<b>100,000</b>	<b>150,000</b>	<b>275,000</b>	<b>475,000</b>
<b><u>Funding Sources:</u></b>									
General Fund	1,130,000	0	0	130,000	100,000	0	150,000	275,000	475,000
Tax-Supported Bond	100,000	100,000	0	0	0	0	0	0	0
Transfer Tax	100,000	0	0	0	0	100,000	0	0	0
<b>Total Funding</b>	<b>1,330,000</b>	<b>100,000</b>	<b>0</b>	<b>130,000</b>	<b>100,000</b>	<b>100,000</b>	<b>150,000</b>	<b>275,000</b>	<b>475,000</b>

Project Title	Information Systems Replacement Program
Project ID	COM011
Full-time Employees	0
Operating Costs	\$0

Project Description	The Information System Replacement Program focuses on the investment in infrastructure hardware and software that provide the foundation on which the business and enterprise systems reside. The Information Technology area currently maintains more than 40 business applications and additionally includes the County's telecommunications (telephone) system.
Project Assumptions	The systems and software serve the departments and typically reach the end of their useful life-expectancy between 3 and 8 years, at which point the systems become increasingly costly to maintain and difficult to exchange information with other systems. Priorities for determining which applications to replace first are driven by age, criticality of the system to operations, and availability of ongoing support from the applications vendor.
Project Justification	The goal of the Information Systems Replacement Program is to keep the County's existing business systems refreshed or replaced on a reasonably expected system lifecycle, so the systems remain useful, operable, and responsive to business needs.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Hardware/Software	1,146,460	146,460	100,000	100,000	100,000	100,000	100,000	100,000	400,000
<b>Total Cost</b>	<b>1,146,460</b>	<b>146,460</b>	<b>100,000</b>	<b>100,000</b>	<b>100,000</b>	<b>100,000</b>	<b>100,000</b>	<b>100,000</b>	<b>400,000</b>
<b>Funding Sources:</b>									
General Fund	946,460	46,460	0	100,000	100,000	100,000	100,000	100,000	400,000
Capital Reserve - General	200,000	100,000	100,000	0	0	0	0	0	0
<b>Total Funding</b>	<b>1,146,460</b>	<b>146,460</b>	<b>100,000</b>	<b>100,000</b>	<b>100,000</b>	<b>100,000</b>	<b>100,000</b>	<b>100,000</b>	<b>400,000</b>



Project Title	Financial System Management & Upgrades
Project ID	COM019
Full-time Employees	0
Operating Costs	\$20,000

Project Description	Integrated financial system (general ledger, human resources, payroll, financial, purchasing, utility, and budget) is used for county-wide operations to process all financials, human resource, payroll, and purchasing functions for the County.
Project Justification	Provides for more efficient use of time and money as systems, employees, and financial management expand to meet demands in the future.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Hardware/Software	427,590	110,590	0	33,000	34,000	34,000	35,000	35,000	146,000
<b>Total Cost</b>	<b>427,590</b>	<b>110,590</b>	<b>0</b>	<b>33,000</b>	<b>34,000</b>	<b>34,000</b>	<b>35,000</b>	<b>35,000</b>	<b>146,000</b>
<b>Funding Sources:</b>									
General Fund	427,590	110,590	0	33,000	34,000	34,000	35,000	35,000	146,000
<b>Total Funding</b>	<b>427,590</b>	<b>110,590</b>	<b>0</b>	<b>33,000</b>	<b>34,000</b>	<b>34,000</b>	<b>35,000</b>	<b>35,000</b>	<b>146,000</b>

Project Title	Broadband Wireless Network Infrastructure
Project ID	COM021
Full-time Employees	0
Operating Costs	\$0

Project Description	The Washington County Broadband Wireless Network Infrastructure System provides fixed broadband (high speed) primary and redundant connectivity for County facilities (i.e. WTP; WwTP, and Pump Station) to network services and a resilient and redundant pathway for the County's fiber network infrastructure. Additionally, this system is available to County divisions and departments and the Washington County Public Network (WCPN) partners that include agencies such as the Washington County Public Schools, Washington County Free Library, 911 Emergency Services, Sheriff's Department, City of Hagerstown Police Department and other City departments and agencies.
Project Assumptions	Wireless communication technology typically reach the end of their useful life-expectancy between 5 and 8 years, at which point the systems become increasingly costly to maintain and difficult to find acceptable replacement technology that meet or exceed bandwidth needs.
Project Justification	The goal of the Washington County Broadband Wireless Network Infrastructure system is to provide for the efficient and cost-effective communication between the sixty (60) plus Environmental Management remote facilities (SCADA) and to provide for a resilient and redundant pathway for the County's fiber network infrastructure.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
Project Costs:									
Hardware/Software	73,745	14,745	11,000	12,000	12,000	12,000	12,000	0	0
Total Cost	73,745	14,745	11,000	12,000	12,000	12,000	12,000	0	0
Funding Sources:									
General Fund	51,745	3,745	0	12,000	12,000	12,000	12,000	0	0
Capital Reserve - General	22,000	11,000	11,000	0	0	0	0	0	0
Total Funding	73,745	14,745	11,000	12,000	12,000	12,000	12,000	0	0

<b>Project Title</b>	<b>General - Equipment and Vehicle Replacement Program</b>
<b>Project ID</b>	VEH008
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$0

**Project Description** This project will serve to replace heavy equipment and vehicles that are beyond their useful life, which are typically the costliest to maintain. In line with the objectives of a well-managed organization, the County wants to establish an equipment and fleet replacement program that will, in time, set the vehicle and equipment replacement cycle in line with best practices, minimizing operating and maintenance costs.

**Project Assumptions** Construction - 11630 - Full Size 4x4 Utility - \$36,300  
Parks & Facilities - 11900 - Dump Truck - \$85,000; 4 Zero-turn Mowers - \$10,000 ea.; Field Overseeder - \$18,000; 2 Tandem Axle Landscape Trailers - \$6,000 ea.  
Special Operations - 11430 - Flat Aluminum Boat Trailer - \$7,600  
EMS Operations - 11520 - 24 CR 2 AED Units - \$1,576 ea.  
Fire Operations - 11525 - 2 Fit-testing Machines - \$9,500 ea.

**Project Justification** The goal of the replacement program is to strike a balance for minimizing replacement costs versus maintenance and fuel costs. The program allows for the purchase of replacement vehicles and equipment used to provide County-wide services within Washington County. The County's equipment and vehicle inventory consists of approximately 2,300 items.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Vehicles	3,929,812	120,812	250,000	359,000	400,000	400,000	400,000	400,000	1,600,000
<b>Total Cost</b>	<b>3,929,812</b>	<b>120,812</b>	<b>250,000</b>	<b>359,000</b>	<b>400,000</b>	<b>400,000</b>	<b>400,000</b>	<b>400,000</b>	<b>1,600,000</b>
<b><u>Funding Sources:</u></b>									
General Fund	3,679,812	120,812	0	359,000	400,000	400,000	400,000	400,000	1,600,000
Capital Reserve - General	250,000	0	250,000	0	0	0	0	0	0
<b>Total Funding</b>	<b>3,929,812</b>	<b>120,812</b>	<b>250,000</b>	<b>359,000</b>	<b>400,000</b>	<b>400,000</b>	<b>400,000</b>	<b>400,000</b>	<b>1,600,000</b>

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# Parks & Recreation

*Parks & Recreation*  
*Capital Improvement Ten Year Summary*  
*Fiscal Year 2021 - 2030*

Page	Project	Total	Prior Appr.	Budget Year		Ten Year Capital Program				
				FY 2021	FY 2022	FY 2023	FY 2024	2025	2026	Future
Project Costs										
<u>Parks &amp; Recreation</u>										
134	BR Capital Equipment Program	368,618	4,618	64,000	25,000	25,000	30,000	30,000	35,000	155,000
135	BR Golf Simulator	20,000	0	20,000	0	0	0	0	0	0
136	Black Rock Club House Renovation	75,000	0	0	75,000	0	0	0	0	0
137	Tennis Court Resurfacing	312,990	42,990	0	30,000	30,000	30,000	30,000	30,000	120,000
138	Park Equipment/Surfacing Replacement, Various Locations	1,917,000	242,000	150,000	150,000	150,000	175,000	175,000	175,000	700,000
139	Parking Lot Repair/Overlay, Various Locations	805,100	305,100	50,000	50,000	50,000	50,000	50,000	50,000	200,000
140	Antietam Creek Water Trail	65,000	0	25,000	40,000	0	0	0	0	0
141	County Parks - Bathroom/ Drinking Fountain Upgrades	250,000	0	50,000	50,000	50,000	50,000	50,000	0	0
142	Doubs Woods Equipment Storage Building	150,000	0	150,000	0	0	0	0	0	0
143	MLK Gymnasium Upgrade	200,000	0	25,000	175,000	0	0	0	0	0
144	Woodland Way Sidewalks and Pathways	40,000	0	40,000	0	0	0	0	0	0
145	Ag Center Show Area Floor	50,000	0	0	0	0	50,000	0	0	0
146	Conococheague Creek Water Trail	210,000	0	0	0	10,000	200,000	0	0	0
147	Marty Snook Dog Park	25,000	0	0	25,000	0	0	0	0	0
148	Marty Snook Park Field Improvements	50,000	0	0	0	0	50,000	0	0	0
149	Marty Snook Park Fitness Trail	300,000	0	0	0	150,000	150,000	0	0	0
150	Marty Snook Park Pool Beach Entrance	100,000	0	0	0	100,000	0	0	0	0
151	North Central County Park	3,286,000	0	0	0	100,000	0	500,000	500,000	2,186,000
152	Recreational Field Bleachers	75,000	0	0	0	0	75,000	0	0	0
153	Regional Park Dog Park	75,000	0	0	0	0	75,000	0	0	0
154	Regional Park Walking/Hiking Trail	200,000	0	0	200,000	0	0	0	0	0
155	Roof Replacements Various Locations	200,000	0	0	0	25,000	25,000	25,000	25,000	100,000
Parks & Recreation Total		8,774,708	594,708	574,000	820,000	690,000	960,000	860,000	815,000	3,461,000
Funding Sources										
General Fund		2,118,618	71,618	0	194,000	252,000	161,000	170,000	340,000	930,000
Capital Reserve - General		142,000	30,000	112,000	0	0	0	0	0	0
Federal Grant		760,000	0	0	120,000	0	150,000	240,000	250,000	0
State Grant		5,754,090	493,090	462,000	506,000	438,000	649,000	450,000	225,000	2,531,000
		8,774,708	594,708	574,000	820,000	690,000	960,000	860,000	815,000	3,461,000

**Project Title** BR Capital Equipment Replacement Program

**Project ID** EQP053

**Full-time Employees** 0

**Operating Costs** \$0



**Project Description** The program allows for the purchase of replacement equipment used for services directly related to Black Rock Golf Course.

**Project Justification** The goal of the replacement program is to balance replacement costs versus maintenance costs. In line with the objectives of a well-managed organization, Black Rock Golf Course strives to establish an equipment replacement program that will set the equipment replacement cycle in line with best practices, minimizing maintenance costs.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Equipment/Furniture	368,618	4,618	64,000	25,000	25,000	30,000	30,000	35,000	155,000
<b>Total Cost</b>	<b>368,618</b>	<b>4,618</b>	<b>64,000</b>	<b>25,000</b>	<b>25,000</b>	<b>30,000</b>	<b>30,000</b>	<b>35,000</b>	<b>155,000</b>
<b><u>Funding Sources:</u></b>									
General Fund	304,618	4,618	0	25,000	25,000	30,000	30,000	35,000	155,000
Capital Reserve - General	64,000	0	64,000	0	0	0	0	0	0
<b>Total Funding</b>	<b>368,618</b>	<b>4,618</b>	<b>64,000</b>	<b>25,000</b>	<b>25,000</b>	<b>30,000</b>	<b>30,000</b>	<b>35,000</b>	<b>155,000</b>

**Project Title**            **Black Rock Golf Simulator**

**Project ID**             REC045

**Full-time Employees**    0

**Operating Costs**        \$0



**Project Description**      Recreational amenity to support training programs and an alternative option during inclement and cold weather.

**Project Assumptions**    Pending Program Open Space Funding.

**Project Justification**    Will increase use and revenue by allowing play during off season and inclement weather.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Hardware/Software	20,000	0	20,000	0	0	0	0	0	0
<b>Total Cost</b>	<b>20,000</b>	<b>0</b>	<b>20,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>Funding Sources:</b>									
Capital Reserve - General	2,000	0	2,000	0	0	0	0	0	0
State Grant	18,000	0	18,000	0	0	0	0	0	0
<b>Total Funding</b>	<b>20,000</b>	<b>0</b>	<b>20,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>



**Project Title**                      **Black Rock Club House Renovation**

**Full-time Employees**            0

**Operating Costs**                  \$0



**Project Description**              Renovation to include mechanical, insulation, restroom upgrades and relocation of Pro Shop.

**Project Assumptions**            Pending Program Open Space funding.

**Project Justification**            Existing building is 30 years old, has not had major renovations done in this time.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	75,000	0	0	75,000	0	0	0	0	0
<b>Total Cost</b>	<b>75,000</b>	<b>0</b>	<b>0</b>	<b>75,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
General Fund	8,000	0	0	8,000	0	0	0	0	0
State Grant	67,000	0	0	67,000	0	0	0	0	0
<b>Total Funding</b>	<b>75,000</b>	<b>0</b>	<b>0</b>	<b>75,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

<b>Project Title</b>	<b>Tennis Courts, Resurfacing</b>
<b>Project ID</b>	LDI046
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$0

<b>Project Description</b>	The project consists of cleaning, crack sealing, repainting and striping of multiple courts over several years.
<b>Project Assumptions</b>	General fund monies or grants other than POS needed to perform remaining tennis court work due to changes at DNR operations. This project is no longer POS eligible.
<b>Project Justification</b>	The tennis courts were rebuilt approximately 8 to 9 years ago and in need of resurfacing which should be done every 5 to 8 years to keep the elements from getting into the subbase and creating more damage. This will be an ongoing project.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	312,990	42,990	0	30,000	30,000	30,000	30,000	30,000	120,000
<b>Total Cost</b>	<b>312,990</b>	<b>42,990</b>	<b>0</b>	<b>30,000</b>	<b>30,000</b>	<b>30,000</b>	<b>30,000</b>	<b>30,000</b>	<b>120,000</b>
<b><u>Funding Sources:</u></b>									
General Fund	312,990	42,990	0	30,000	30,000	30,000	30,000	30,000	120,000
<b>Total Funding</b>	<b>312,990</b>	<b>42,990</b>	<b>0</b>	<b>30,000</b>	<b>30,000</b>	<b>30,000</b>	<b>30,000</b>	<b>30,000</b>	<b>120,000</b>

**Project Title**                      **Park Equipment/Surfacing Replacement - Various Locations**

**Project ID**                              REC037

**Full-time Employees**              0

**Operating Costs**                    \$0

**Project Description**              The project will replace the existing playground equipment at designated locations.

**Project Assumptions**            Pending POS and/or other funding sources.

**Project Justification**            The existing playground structure is 20 plus years old and is showing signs of degradation due to weather and usage.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Equipment/Furniture	1,917,000	242,000	150,000	150,000	150,000	175,000	175,000	175,000	700,000
<b>Total Cost</b>	<b>1,917,000</b>	<b>242,000</b>	<b>150,000</b>	<b>150,000</b>	<b>150,000</b>	<b>175,000</b>	<b>175,000</b>	<b>175,000</b>	<b>700,000</b>
<b><u>Funding Sources:</u></b>									
General Fund	165,000	9,000	0	15,000	15,000	18,000	18,000	18,000	72,000
Capital Reserve - General	30,000	15,000	15,000	0	0	0	0	0	0
State Grant	1,722,000	218,000	135,000	135,000	135,000	157,000	157,000	157,000	628,000
<b>Total Funding</b>	<b>1,917,000</b>	<b>242,000</b>	<b>150,000</b>	<b>150,000</b>	<b>150,000</b>	<b>175,000</b>	<b>175,000</b>	<b>175,000</b>	<b>700,000</b>

**Project Title**                      **Repair/Overlay Parking Lots - Various Locations**

**Project ID**                              REC042

**Full-time Employees**              0

**Operating Costs**                    \$0

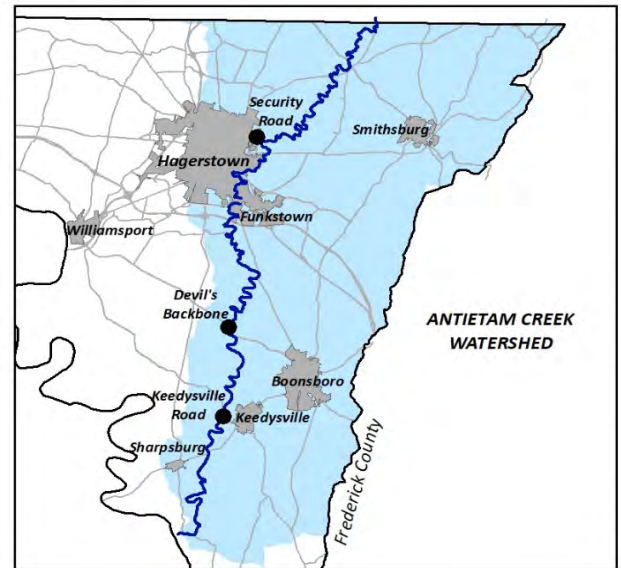
**Project Description**              Pen Mark Park and Marty Snook Pool - Parking Lot Repair/Overlay.

**Project Assumptions**            Pending POS and/or other funding sources.

**Project Justification**            The existing locations are 20 plus years old and in poor condition.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	805,100	305,100	50,000	50,000	50,000	50,000	50,000	50,000	200,000
<b>Total Cost</b>	<b>805,100</b>	<b>305,100</b>	<b>50,000</b>	<b>50,000</b>	<b>50,000</b>	<b>50,000</b>	<b>50,000</b>	<b>50,000</b>	<b>200,000</b>
<b><u>Funding Sources:</u></b>									
General Fund	60,010	15,010	0	5,000	5,000	5,000	5,000	5,000	20,000
Capital Reserve - General	20,000	15,000	5,000	0	0	0	0	0	0
State Grant	725,090	275,090	45,000	45,000	45,000	45,000	45,000	45,000	180,000
<b>Total Funding</b>	<b>805,100</b>	<b>305,100</b>	<b>50,000</b>	<b>50,000</b>	<b>50,000</b>	<b>50,000</b>	<b>50,000</b>	<b>50,000</b>	<b>200,000</b>

<b>Project Title</b>	<b>Antietam Creek Water Trail</b>
<b>Project ID</b>	REC044
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$0



<b>Project Description</b>	The project will establish the Antietam Water Trail, which may include property acquisition, public access improvements, and installing new boat access. Current identified sections for improvement include Security Road (FY21-FY22), Devil's Backbone (FY21), and Keedysville Road (FY22).
<b>Project Assumptions</b>	Pending Program Open Space and Waterway Improvement Fund grant funding.
<b>Project Justification</b>	This water trail would improve recreational access to the Antietam Creek.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	50,000	0	10,000	40,000	0	0	0	0	0
Land Acquisition	15,000	0	15,000	0	0	0	0	0	0
<b>Total Cost</b>	<b>65,000</b>	<b>0</b>	<b>25,000</b>	<b>40,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
General Fund	6,000	0	0	6,000	0	0	0	0	0
State Grant	59,000	0	25,000	34,000	0	0	0	0	0
<b>Total Funding</b>	<b>65,000</b>	<b>0</b>	<b>25,000</b>	<b>40,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

**Project Title** County Parks Bathroom/Drinking Fountain Upgrades

**Project ID** REC046

**Full-time Employees** 0

**Operating Costs** \$0

**Project Description** Bathroom/drinking fountain upgrades in various locations. Priority list Marty Snook, Regional and Pen Mar Parks.

**Project Assumptions** Pending Program Open Space funding.

**Project Justification** The pavilions in Marty Snook, Regional and Pen Mar Parks were built 20 to 30 years ago. Little to no updates have been done in the attached bathrooms since construction.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	250,000	0	50,000	50,000	50,000	50,000	50,000	0	0
<b>Total Cost</b>	<b>250,000</b>	<b>0</b>	<b>50,000</b>	<b>50,000</b>	<b>50,000</b>	<b>50,000</b>	<b>50,000</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
General Fund	20,000	0	0	5,000	5,000	5,000	5,000	0	0
Capital Reserve - General	5,000	0	5,000	0	0	0	0	0	0
State Grant	225,000	0	45,000	45,000	45,000	45,000	45,000	0	0
<b>Total Funding</b>	<b>250,000</b>	<b>0</b>	<b>50,000</b>	<b>50,000</b>	<b>50,000</b>	<b>50,000</b>	<b>50,000</b>	<b>0</b>	<b>0</b>

**Project Title**            **Doub's Woods Equipment Storage Building**

**Project ID**                REC047

**Full-time Employees**    0

**Operating Costs**         \$0



**Project Description**      Construction of a 40 x 80 building for department equipment storage.

**Project Assumptions**    Pending Program Open Space funding.

**Project Justification**     Building needed to store department equipment/vehicles presently stored outside.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Construction	150,000	0	150,000	0	0	0	0	0	0
<b>Total Cost</b>	<b>150,000</b>	<b>0</b>	<b>150,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
Capital Reserve - General	15,000	0	15,000	0	0	0	0	0	0
State Grant	135,000	0	135,000	0	0	0	0	0	0
<b>Total Funding</b>	<b>150,000</b>	<b>0</b>	<b>150,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

**Project Title** MLK Gymnasium Upgrades

**Project ID** REC048

**Full-time Employees** 0

**Operating Costs** \$0



**Project Description** Gymnasium upgrades to include new flooring, lighting and HVAC.

**Project Assumptions** Pending Program Open Space funding.

**Project Justification** The current Gymnasium at the Martin Luther King Building was built in 1950. Very little has been done over the years to upgrade this space. New flooring, amenities, bathroom, drinking fountains, ADA accessibility, security, lighting and HVAC would increase the use of this space.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	200,000	0	25,000	175,000	0	0	0	0	0
<b>Total Cost</b>	<b>200,000</b>	<b>0</b>	<b>25,000</b>	<b>175,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>Funding Sources:</b>									
General Fund	17,000	0	0	17,000	0	0	0	0	0
Capital Reserve - General	2,000	0	2,000	0	0	0	0	0	0
State Grant	181,000	0	23,000	158,000	0	0	0	0	0
<b>Total Funding</b>	<b>200,000</b>	<b>0</b>	<b>25,000</b>	<b>175,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

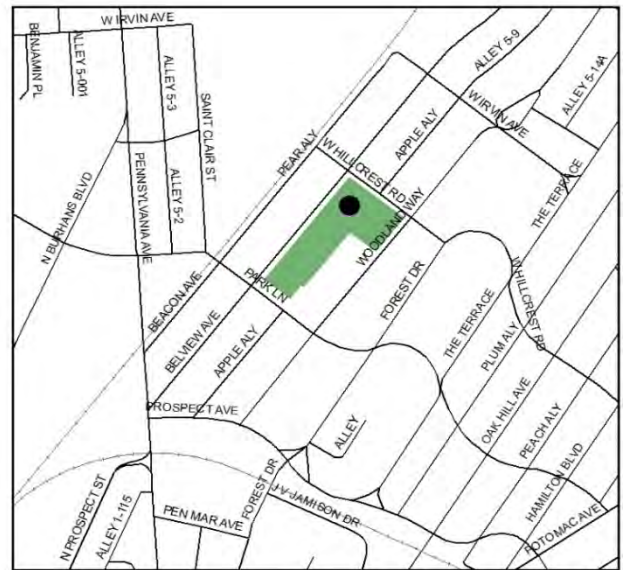


**Project Title** Woodland Way Sidewalks and Pathways

**Project ID** REC049

**Full-time Employees** 0

**Operating Costs** \$0



**Project Description** Replace portions of existing asphalt pathways and extend sidewalk along Belview Avenue. Resurface asphalt pathways throughout park.

**Project Assumptions** Pending Program Open Space Funding.

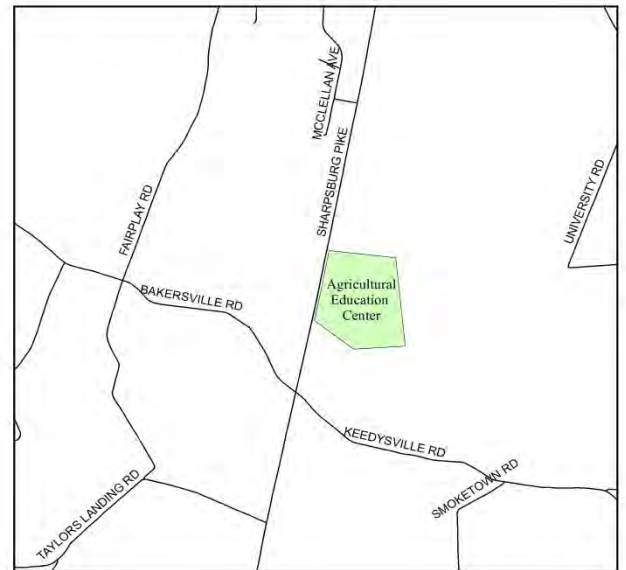
**Project Justification** The current sidewalks and pathways are past their useful life and must be replaced for safety reasons.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Construction	40,000	0	40,000	0	0	0	0	0	0
<b>Total Cost</b>	<b>40,000</b>	<b>0</b>	<b>40,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
Capital Reserve - General	4,000	0	4,000	0	0	0	0	0	0
State Grant	36,000	0	36,000	0	0	0	0	0	0
<b>Total Funding</b>	<b>40,000</b>	<b>0</b>	<b>40,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

**Project Title** Ag Center Show Area Floor

**Full-time Employees** 0

**Operating Costs** \$0



**Project Description** Installation of a concrete floor in the existing Show Barn at the Ag Center.

**Project Assumptions** Pending Civil War Heritage Area funding.

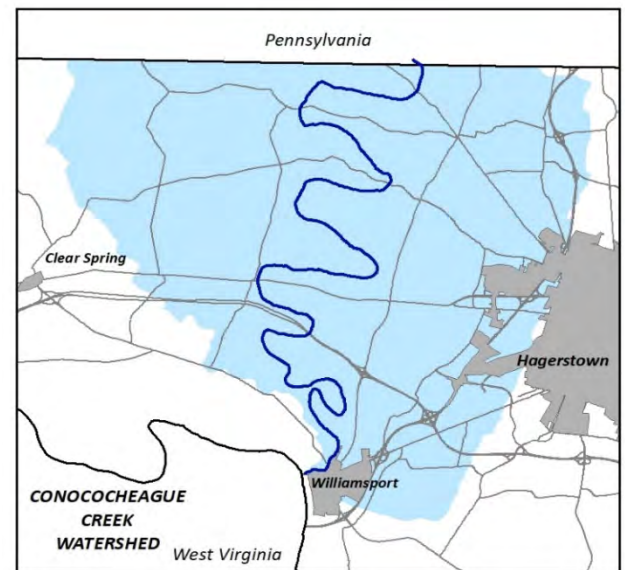
**Project Justification** The current floor in this building is dirt only. The installation of concrete in this building would make the space usable to more events. Concrete would also make cleanup of this building easier.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Construction	50,000	0	0	0	0	50,000	0	0	0
<b>Total Cost</b>	<b>50,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>50,000</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
General Fund	12,000	0	0	0	0	12,000	0	0	0
State Grant	38,000	0	0	0	0	38,000	0	0	0
<b>Total Funding</b>	<b>50,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>50,000</b>	<b>0</b>	<b>0</b>	<b>0</b>

**Project Title** Conococheague Creek Water Trail

**Full-time Employees** 0

**Operating Costs** \$0



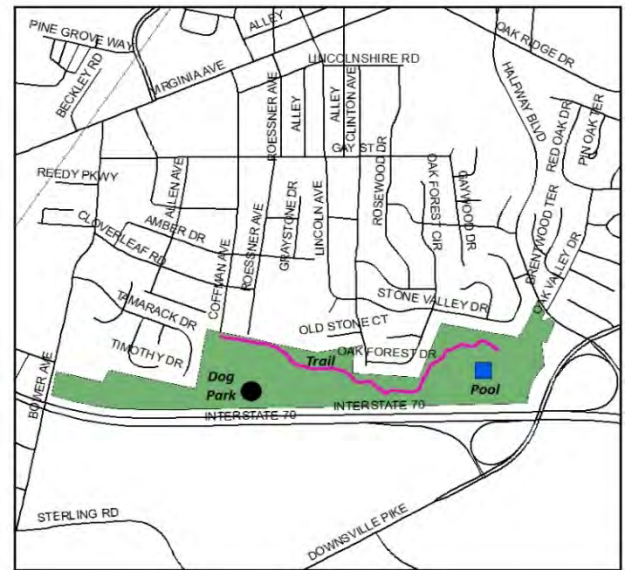
**Project Description** Development of Conococheague Water Trail and creation of public access and parking locations.

**Project Assumptions** Pending Waterway Improvement funding.

**Project Justification** This boat access would establish the Conococheague Creek Water Trail. This water trail would improve recreational access to the Conococheague Creek.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Construction	200,000	0	0	0	0	200,000	0	0	0
Other	10,000	0	0	0	10,000	0	0	0	0
<b>Total Cost</b>	<b>210,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>10,000</b>	<b>200,000</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
General Fund	40,000	0	0	0	0	40,000	0	0	0
State Grant	170,000	0	0	0	10,000	160,000	0	0	0
<b>Total Funding</b>	<b>210,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>10,000</b>	<b>200,000</b>	<b>0</b>	<b>0</b>	<b>0</b>

<b>Project Title</b>	<b>Marty Snook Park Dog Park</b>
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$0



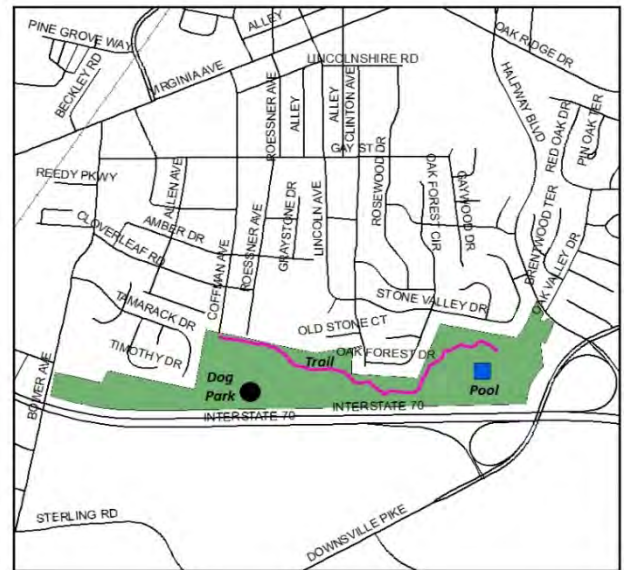
<b>Project Description</b>	Amenities for the Dog Park at Marty Snook Park.
<b>Project Assumptions</b>	Pending Program Open Space Funding.
<b>Project Justification</b>	The current Dog Park is in need of shade structures, seating and access to water.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Construction	25,000	0	0	25,000	0	0	0	0	0
<b>Total Cost</b>	<b>25,000</b>	<b>0</b>	<b>0</b>	<b>25,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
General Fund	3,000	0	0	3,000	0	0	0	0	0
State Grant	22,000	0	0	22,000	0	0	0	0	0
<b>Total Funding</b>	<b>25,000</b>	<b>0</b>	<b>0</b>	<b>25,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

**Project Title** Marty Snook Park Field Improvements

**Full-time Employees** 0

**Operating Costs** \$0



**Project Description** Athletic field upgrades.

**Project Assumptions** Pending Program Open Space funding.

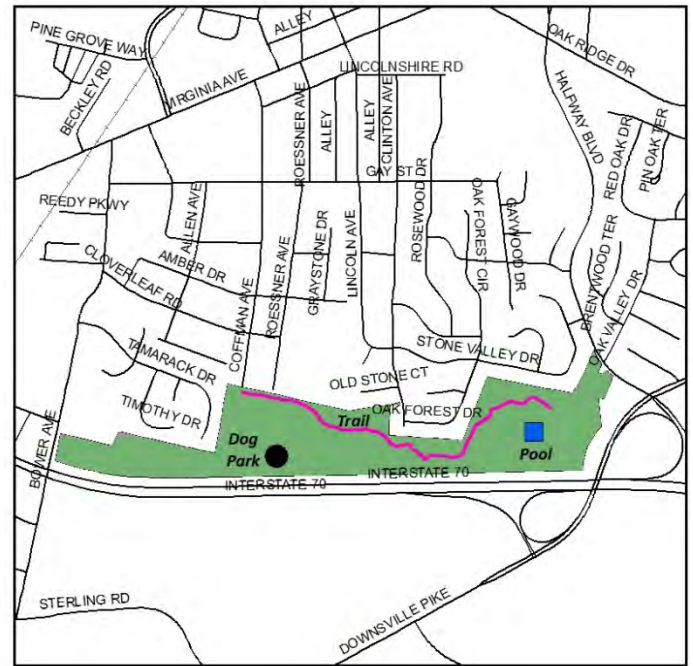
**Project Justification** Athletic field upgrades including playing surfaces, fencing, sports amenities, spectating areas, and irrigation.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Construction	50,000	0	0	0	0	50,000	0	0	0
<b>Total Cost</b>	<b>50,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>50,000</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
General Fund	5,000	0	0	0	0	5,000	0	0	0
State Grant	45,000	0	0	0	0	45,000	0	0	0
<b>Total Funding</b>	<b>50,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>50,000</b>	<b>0</b>	<b>0</b>	<b>0</b>

**Project Title** Marty Snook Park Fitness Trail

**Full-time Employees** 0

**Operating Costs** \$0



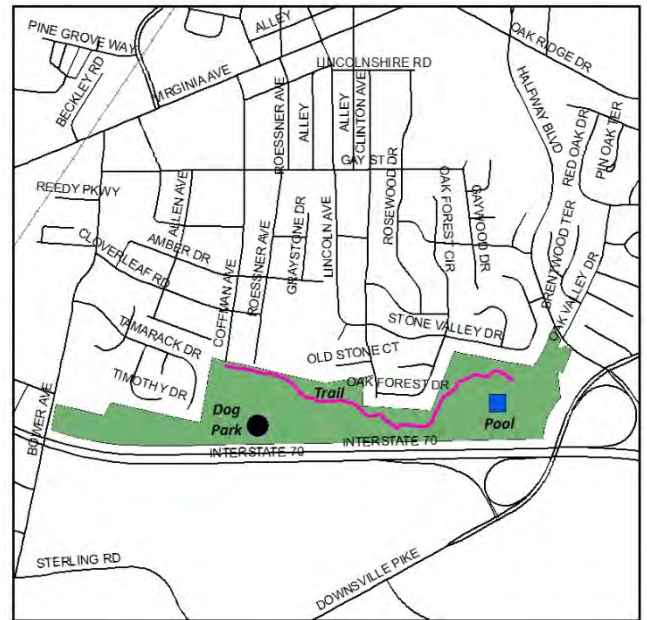
**Project Description** Expansion of the Fitness Trail at Marty Snook Park.

**Project Assumptions** Pending Land and Water Conservation funding.

**Project Justification** Installation of fitness station and equipment at locations along existing trail. Includes trail upgrades and improvements.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Construction	300,000	0	0	0	150,000	150,000	0	0	0
<b>Total Cost</b>	<b>300,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>150,000</b>	<b>150,000</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
General Fund	150,000	0	0	0	150,000	0	0	0	0
Federal Grant	150,000	0	0	0	0	150,000	0	0	0
<b>Total Funding</b>	<b>300,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>150,000</b>	<b>150,000</b>	<b>0</b>	<b>0</b>	<b>0</b>

<b>Project Title</b>	<b>Marty Snook Park Pool Accessible Entrance</b>
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$0



<b>Project Description</b>	Create an Accessible entrance into the existing pool at Marty Snook Park.
<b>Project Assumptions</b>	Pending Program Open Space funding.
<b>Project Justification</b>	Creation of a beach style walk in entrance at the pool to improve entry for all ages and upgrade ADA access.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Construction	100,000	0	0	0	100,000	0	0	0	0
<b>Total Cost</b>	<b>100,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>100,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
General Fund	10,000	0	0	0	10,000	0	0	0	0
State Grant	90,000	0	0	0	90,000	0	0	0	0
<b>Total Funding</b>	<b>100,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>100,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>



**Project Title** North Central County Park

**Full-time Employees** 2

**Operating Costs** \$81,000



**Project Description** This project involves a multi-phased development of a regional park along the north side of Eastern Boulevard Extended. Phase 1 work will include construction of an entrance road, parking lot, grading for soccer/ball fields, and an 8' wide paved pedestrian/bike path with street/park lighting. Future phases include pavilion buildings, restrooms, playground equipment and other park and recreation amenities.

**Project Assumptions** This project assumes that Eastern Blvd Extended (Project 21) is completed.

**Project Justification** This project provides additional recreational opportunities in the north end of the County. The land is currently under lease for a 99-year period.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
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**Project Costs:**

Construction	3,000,000	0	0	0	0	0	500,000	500,000	2,000,000
Engineering and Design	100,000	0	0	0	100,000	0	0	0	0
Inspection	186,000	0	0	0	0	0	0	0	186,000
<b>Total Cost</b>	<b>3,286,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>100,000</b>	<b>0</b>	<b>500,000</b>	<b>500,000</b>	<b>2,186,000</b>

**Funding Sources:**

General Fund	895,000	0	0	0	10,000	0	80,000	250,000	555,000
Federal Grant	490,000	0	0	0	0	0	240,000	250,000	0
State Grant	1,901,000	0	0	0	90,000	0	180,000	0	1,631,000
<b>Total Funding</b>	<b>3,286,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>100,000</b>	<b>0</b>	<b>500,000</b>	<b>500,000</b>	<b>2,186,000</b>



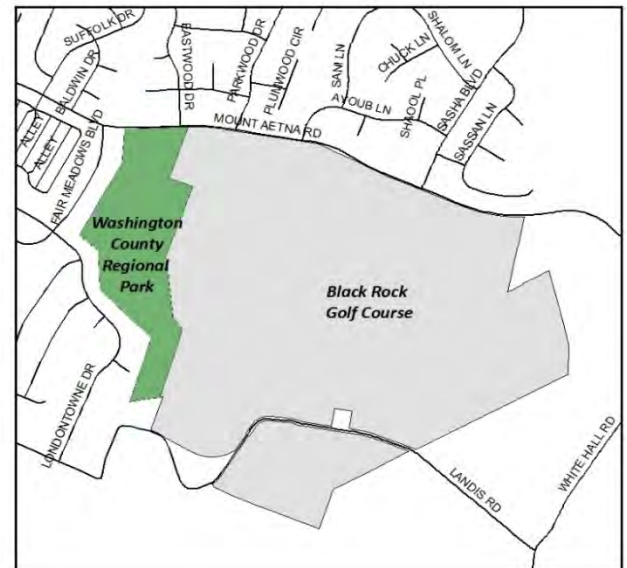
**Project Title                      Recreational Field Bleachers****Full-time Employees**                      0**Operating Costs**                      \$0**Project Description**                      New portable field bleachers that would be used in multiple parks.**Project Assumptions**                      Pending Program Open Space funding.**Project Justification**                      Replace existing aging bleachers. Existing bleaches are beyond repair and no longer meet code requirements.  
Replacement would allow use at multiple locations.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Other	75,000	0	0	0	0	75,000	0	0	0
<b>Total Cost</b>	<b>75,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>75,000</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
General Fund	7,000	0	0	0	0	7,000	0	0	0
State Grant	68,000	0	0	0	0	68,000	0	0	0
<b>Total Funding</b>	<b>75,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>75,000</b>	<b>0</b>	<b>0</b>	<b>0</b>

**Project Title**                      **Regional Park Dog Park**

**Full-time Employees**            0

**Operating Costs**                 \$0



**Project Description**            New dog park facility including fencing, facility amenities, landscaping and water line extension.

**Project Assumptions**          Pending Program Open Space funding.

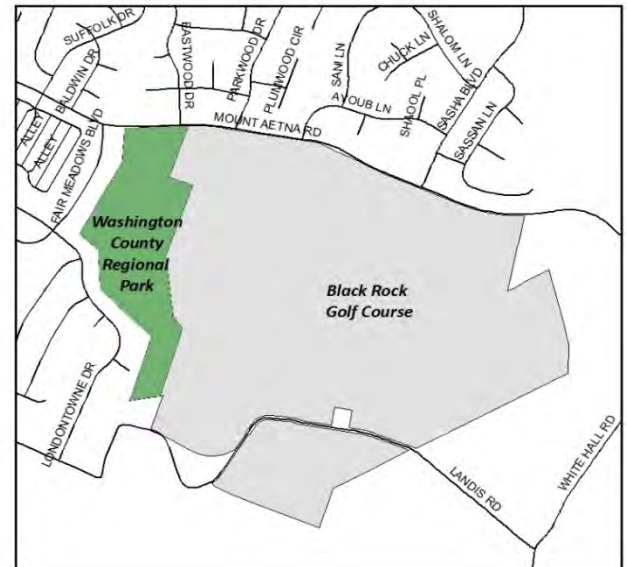
**Project Justification**          Installation of a Dog Park at Regional Park is needed due to the increased use by dog owners.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	75,000	0	0	0	0	75,000	0	0	0
<b>Total Cost</b>	<b>75,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>75,000</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
General Fund	7,000	0	0	0	0	7,000	0	0	0
State Grant	68,000	0	0	0	0	68,000	0	0	0
<b>Total Funding</b>	<b>75,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>75,000</b>	<b>0</b>	<b>0</b>	<b>0</b>

**Project Title**                      **Regional Park Walking and Hiking Trail**

**Full-time Employees**            0

**Operating Costs**                 \$0



**Project Description**            Walking and Hiking Trail at Regional Park

**Project Assumptions**          Pending State Highway Administration Recreational Trail Grant.

**Project Justification**          The need exists for a walking and hiking trail at Regional Park. Currently the wooded area at the park is used for walking and hiking. With the exception of the Disc Golf trails no others exist in this area. The addition of trails at Regional Park would provide the users a safe place to walk and hike.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	200,000	0	0	200,000	0	0	0	0	0
<b>Total Cost</b>	<b>200,000</b>	<b>0</b>	<b>0</b>	<b>200,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
General Fund	80,000	0	0	80,000	0	0	0	0	0
Federal Grant	120,000	0	0	120,000	0	0	0	0	0
<b>Total Funding</b>	<b>200,000</b>	<b>0</b>	<b>0</b>	<b>200,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

**Project Title**                      **Roof Replacements at Various Locations**

**Full-time Employees**                      0

**Operating Costs**                      \$0

**Project Description**                      Roof replacements at various locations within the Parks.

**Project Assumptions**                      Pending Program Open Space funding.

**Project Justification**                      Buildings within the Parks system are aging to the point that major repairs are needed.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	200,000	0	0	0	25,000	25,000	25,000	25,000	100,000
<b>Total Cost</b>	<b>200,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>25,000</b>	<b>25,000</b>	<b>25,000</b>	<b>25,000</b>	<b>100,000</b>
<b><u>Funding Sources:</u></b>									
General Fund	16,000	0	0	0	2,000	2,000	2,000	2,000	8,000
State Grant	184,000	0	0	0	23,000	23,000	23,000	23,000	92,000
<b>Total Funding</b>	<b>200,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>25,000</b>	<b>25,000</b>	<b>25,000</b>	<b>25,000</b>	<b>100,000</b>

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# Public Safety

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*Public Safety*  
*Capital Improvement Ten Year Summary*  
*Fiscal Year 2021 - 2030*

Page	Project	Total	Prior Appr.	Budget Year		Ten Year Capital Program				
				FY 2021	FY 2022	FY 2023	FY 2024	FY 2025	FY 2026	Future
Project Costs										
Public Safety										
158	Detention Center - Systemic Projects	3,029,323	582,323	0	450,000	450,000	300,000	270,000	20,000	957,000
159	Police & EMS Training Facility	12,000,000	7,300,000	1,900,000	1,400,000	1,400,000	0	0	0	0
160	Communication Tower(s) Various	642,526	178,526	0	110,000	0	114,000	0	118,000	122,000
161	Motorola Portable Radio Replacement Program	1,268,821	106,821	108,000	110,000	112,000	114,000	116,000	118,000	484,000
162	Law Enforcement - Vehicle & Equipment Replacement Program	7,354,715	449,715	520,000	650,000	660,000	650,000	725,000	725,000	2,975,000
163	Emergency Services Equipment & Vehicle Program	2,357,183	622,183	75,000	150,000	160,000	150,000	200,000	200,000	800,000
164	911 Center Building Expansion	1,800,000	0	0	0	0	0	250,000	1,200,000	350,000
165	Class A Burn Building - HFD Training Center	500,000	0	0	500,000	0	0	0	0	0
166	P25 UHF Public Safety Radio Communications System Upgrade	5,800,000	0	0	200,000	700,000	700,000	700,000	700,000	2,800,000
167	Patrol Services Relocation Renovation	5,180,000	0	0	0	0	500,000	500,000	500,000	3,680,000
Public Safety Total		39,932,568	9,239,568	2,603,000	3,570,000	3,482,000	2,528,000	2,761,000	3,581,000	12,168,000
Funding Sources										
	General Fund	27,772,245	5,312,245	800,000	2,420,000	2,232,000	2,228,000	2,511,000	2,381,000	9,888,000
	Tax-Supported Bond	8,109,752	3,379,752	100,000	700,000	800,000	0	250,000	1,200,000	1,680,000
	Transfer Tax	1,540,571	90,571	0	450,000	450,000	300,000	0	0	250,000
	Capital Reserve - General	1,159,000	456,000	703,000	0	0	0	0	0	0
	State Grant	1,350,000	0	1,000,000	0	0	0	0	0	350,000
	Contributions	1,000	1,000	0	0	0	0	0	0	0
		39,932,568	9,239,568	2,603,000	3,570,000	3,482,000	2,528,000	2,761,000	3,581,000	12,168,000

<b>Project Title</b>	<b>Detention Center - Systemic Projects</b>
<b>Project ID</b>	BLD089
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$0



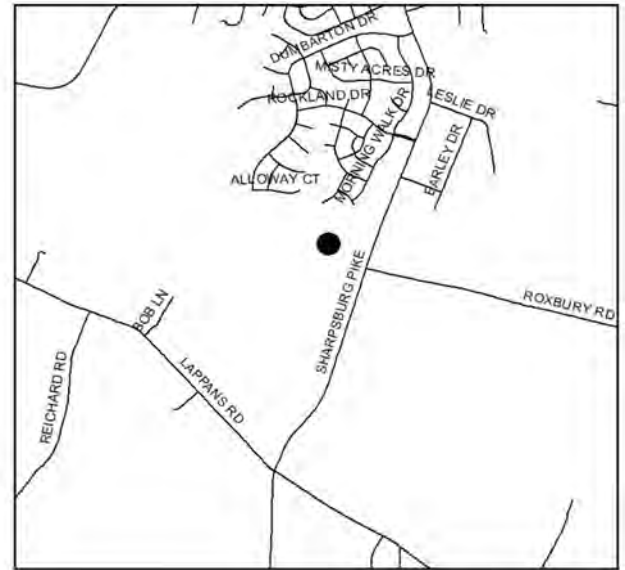
**Project Description** This project allows for major repairs and/or building improvements as needed for the Detention Center.

**Project Justification** The Detention Center is an aging facility that is starting to need major repairs to maintain functionality and compliance.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Other	3,029,323	582,323	0	450,000	450,000	300,000	270,000	20,000	957,000
<b>Total Cost</b>	<b>3,029,323</b>	<b>582,323</b>	<b>0</b>	<b>450,000</b>	<b>450,000</b>	<b>300,000</b>	<b>270,000</b>	<b>20,000</b>	<b>957,000</b>
<b><u>Funding Sources:</u></b>									
General Fund	1,349,000	352,000	0	0	0	0	270,000	20,000	707,000
Tax-Supported Bond	139,752	139,752	0	0	0	0	0	0	0
Transfer Tax	1,540,571	90,571	0	450,000	450,000	300,000	0	0	250,000
<b>Total Funding</b>	<b>3,029,323</b>	<b>582,323</b>	<b>0</b>	<b>450,000</b>	<b>450,000</b>	<b>300,000</b>	<b>270,000</b>	<b>20,000</b>	<b>957,000</b>



<b>Project Title</b>	<b>Police, Fire and Emergency Services Training Facility</b>
<b>Project ID</b>	BLD093
<b>Full-time Employees</b>	3
<b>Operating Costs</b>	\$500,000



<b>Project Description</b>	As the need for police, fire, and emergency services continues to grow, the need for a facility for training emergency personnel grows as well. Such a facility would be centralized to the area, easily accessible, and utilized by state, county and local police departments, correctional guards, and security guards. Classroom training for fire and emergency services departments, that predominantly takes place currently in local fire houses, would be held in this facility. With the potential to utilize previously owned County property, the facility would allow for local law enforcement and Fire/EMS programs to migrate from the HCC facilities and have a designated public safety campus. The project would be constructed over several phases ranging from the main building to other necessary training structures.
<b>Project Assumptions</b>	Assumes the use of speed camera revenue to pay for construction costs for years 2019-2024. The County is also seeking State grants.
<b>Project Justification</b>	This facility will provide a needed local service to the large number of police, fire, correctional, and other emergency services personnel serving in and around Washington County, as well as provide training to people planning to enter those fields. As the area continues to grow, this will help meet the increasing demand for trained EMS personnel.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	10,178,000	5,628,000	1,900,000	1,400,000	1,250,000	0	0	0	0
Engineering and Design	612,000	612,000	0	0	0	0	0	0	0
Equipment/Furniture	1,060,000	1,060,000	0	0	0	0	0	0	0
Inspection	150,000	0	0	0	150,000	0	0	0	0
<b>Total Cost</b>	<b>12,000,000</b>	<b>7,300,000</b>	<b>1,900,000</b>	<b>1,400,000</b>	<b>1,400,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
General Fund	6,160,000	4,060,000	800,000	700,000	600,000	0	0	0	0
Tax-Supported Bond	4,840,000	3,240,000	100,000	700,000	800,000	0	0	0	0
State Grant	1,000,000	0	1,000,000	0	0	0	0	0	0
<b>Total Funding</b>	<b>12,000,000</b>	<b>7,300,000</b>	<b>1,900,000</b>	<b>1,400,000</b>	<b>1,400,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

Project Title	Communication Tower(s) - Various Locations
Project ID	COM027
Full-time Employees	0
Operating Costs	\$0

Project Description	The project includes the construction of new and replacement of old public safety communication towers at various sites, and miscellaneous add on equipment.
Project Assumptions	Towers will hold public safety communications equipment and County wireless broad band equipment.
Project Justification	Existing towers were constructed in the 1970's and are structurally and functionally inadequate.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Construction	642,526	178,526	0	110,000	0	114,000	0	118,000	122,000
<b>Total Cost</b>	<b>642,526</b>	<b>178,526</b>	<b>0</b>	<b>110,000</b>	<b>0</b>	<b>114,000</b>	<b>0</b>	<b>118,000</b>	<b>122,000</b>
<b><u>Funding Sources:</u></b>									
General Fund	592,526	128,526	0	110,000	0	114,000	0	118,000	122,000
Capital Reserve - General	50,000	50,000	0	0	0	0	0	0	0
<b>Total Funding</b>	<b>642,526</b>	<b>178,526</b>	<b>0</b>	<b>110,000</b>	<b>0</b>	<b>114,000</b>	<b>0</b>	<b>118,000</b>	<b>122,000</b>

Project Title	Motorola Portable Radio Replacement Program
Project ID	EQP060
Full-time Employees	0
Operating Costs	\$0

Project Description	The Motorola Portable Radio Replacement Program focuses on the investment in Motorola portable radios used exclusively by Washington County government's public safety first responders (Sheriff's Office & Emergency Services personnel) to provide personal communication infrastructure in conjunction with the County's Motorola UHF Public Safety trunked radio system.
Project Assumptions	The Motorola portable radios provide the County's first responders with reliable communications to the County's 911 Dispatch Center and with other public safety talk groups and have a useful life-expectancy between 12 and 13 years, at which point these radios become increasingly costly to maintain and to remain interoperable with the County's Motorola UHF Public Safety trunked radio system, which is refreshed every three to five years. Ninety percent of the current portable radio inventory was purchased during the initial phase of the implementation project in 2007.
Project Justification	The goal of the Motorola Portable Radio Replacement Program is to keep the County's Motorola UHF Public Safety trunked radio system useful, operable and responsive to the County's public safety needs and interoperable with Maryland's new 700 MHz state-wide public safety communications network.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Equipment/Furniture	1,268,821	106,821	108,000	110,000	112,000	114,000	116,000	118,000	484,000
<b>Total Cost</b>	<b>1,268,821</b>	<b>106,821</b>	<b>108,000</b>	<b>110,000</b>	<b>112,000</b>	<b>114,000</b>	<b>116,000</b>	<b>118,000</b>	<b>484,000</b>
<b>Funding Sources:</b>									
General Fund	1,054,821	821	0	110,000	112,000	114,000	116,000	118,000	484,000
Capital Reserve - General	214,000	106,000	108,000	0	0	0	0	0	0
<b>Total Funding</b>	<b>1,268,821</b>	<b>106,821</b>	<b>108,000</b>	<b>110,000</b>	<b>112,000</b>	<b>114,000</b>	<b>116,000</b>	<b>118,000</b>	<b>484,000</b>

Project Title	Law Enforcement - Vehicle and Equipment Replacement Program
Project ID	VEH006
Full-time Employees	0
Operating Costs	\$0

**Project Description**      This project is for the replacement of public safety fleet inventory.

**Project Justification**      The Sheriff's operations has approximately 136 vehicles maintained by its fleet maintenance department. A vehicle replacement program has been implemented and designed to reduce maintenance and operational downtime due to vehicle age, mileage, and repair cost versus book value.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Vehicles	7,354,715	449,715	520,000	650,000	660,000	650,000	725,000	725,000	2,975,000
<b>Total Cost</b>	<b>7,354,715</b>	<b>449,715</b>	<b>520,000</b>	<b>650,000</b>	<b>660,000</b>	<b>650,000</b>	<b>725,000</b>	<b>725,000</b>	<b>2,975,000</b>
<b><u>Funding Sources:</u></b>									
General Fund	6,533,715	148,715	0	650,000	660,000	650,000	725,000	725,000	2,975,000
Capital Reserve - General	820,000	300,000	520,000	0	0	0	0	0	0
Contributions	1,000	1,000	0	0	0	0	0	0	0
<b>Total Funding</b>	<b>7,354,715</b>	<b>449,715</b>	<b>520,000</b>	<b>650,000</b>	<b>660,000</b>	<b>650,000</b>	<b>725,000</b>	<b>725,000</b>	<b>2,975,000</b>

Project Title	Emergency Services Equipment & Vehicle Program
Project ID	VEH009
Full-time Employees	0
Operating Costs	\$0

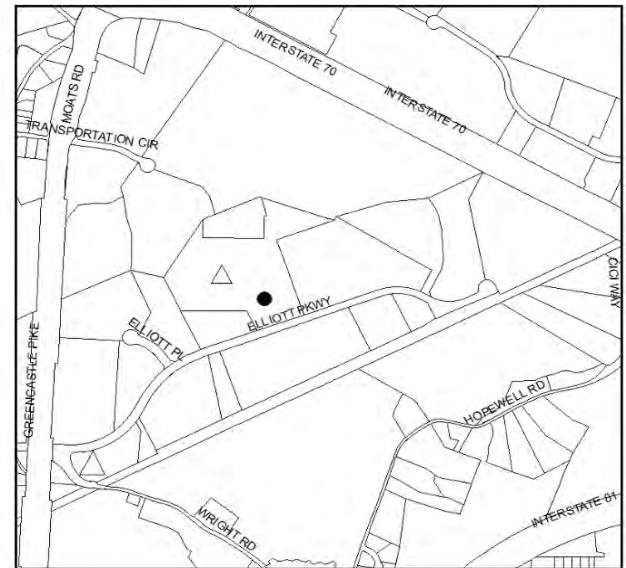
Project Description	The project is a 10-year capital equipment and vehicle replacement program for the County's Division of Emergency Services. DES needs to have a sustainable plan to replace critical vehicles utilized for emergency response.
Project Assumptions	Special Operations - 11430 - Rescue Boat (FY21).
Project Justification	Emergency services capital equipment and vehicle program is designed to graduate funding over a period of time, reduce maintenance costs and operational downtime due to age, mileage, and repair costs versus book values.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Vehicles	2,357,183	622,183	75,000	150,000	160,000	150,000	200,000	200,000	800,000
<b>Total Cost</b>	<b>2,357,183</b>	<b>622,183</b>	<b>75,000</b>	<b>150,000</b>	<b>160,000</b>	<b>150,000</b>	<b>200,000</b>	<b>200,000</b>	<b>800,000</b>
<b>Funding Sources:</b>									
General Fund	2,282,183	622,183	0	150,000	160,000	150,000	200,000	200,000	800,000
Capital Reserve - General	75,000	0	75,000	0	0	0	0	0	0
<b>Total Funding</b>	<b>2,357,183</b>	<b>622,183</b>	<b>75,000</b>	<b>150,000</b>	<b>160,000</b>	<b>150,000</b>	<b>200,000</b>	<b>200,000</b>	<b>800,000</b>

**Project Title**                      **911 Emergency Communication Center Building Expansion**

**Full-time Employees**        0

**Operating Costs**             \$0



**Project Description**        The project involves the building expansion of the existing 911 Emergency Communication Center at Elliott Parkway. As the need for Fire, Police, and Emergency Services continue to grow, the need for the construction of a new or expanded ECC is needed in Washington County. The volume of calls the ECC receives is growing between 3-5% per year for the last 5 years. Currently the ECC receives close to 300,000 calls per year. Currently the ECC has 12 call taking/dispatch stations and with the proposed 32'x48' expansion the area would accommodate 24 call taking/dispatch stations to handle the expected volume of calls received. With the implementation of Next Generation 911 in 2020, the volume of calls will increase in the ECC.

**Project Assumptions**      Partial funding will be awarded to Washington County by the State Emergency Numbers System Board (ENSB). The ENSB awards funds based on population of the county. The ENSB also awarded grants to enhance 911 call taking ability.

**Project Justification**        This facility will expand the call taking capabilities for the Washington County ECC. As the call volume and population of Washington County grows over the next decade, this facility will be sufficient to accept the growth of the staff at the ECC. In addition, the renovated facility would also benefit the Emergency Operation Center.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
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**Project Costs:**

Construction	1,100,000	0	0	0	0	0	0	1,100,000	0
Engineering and Design	250,000	0	0	0	0	0	250,000	0	0
Equipment/Furniture	350,000	0	0	0	0	0	0	0	350,000
Inspection	100,000	0	0	0	0	0	0	100,000	0
<b>Total Cost</b>	<b>1,800,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>250,000</b>	<b>1,200,000</b>	<b>350,000</b>

**Funding Sources:**

Tax-Supported Bond	1,450,000	0	0	0	0	0	250,000	1,200,000	0
State Grant	350,000	0	0	0	0	0	0	0	350,000
<b>Total Funding</b>	<b>1,800,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>250,000</b>	<b>1,200,000</b>	<b>350,000</b>

<b>Project Title</b>	<b>Class A Burn Building - HFD Training Center</b>
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$0



<b>Project Description</b>	This project assumes a 50/50 split in cost associated with the purchase and installation of a modular constructed class A burn building at the Bowman Ave. HFD Training site. Total cost of the building is expected to be \$1 million; the \$500,000 budgeted represents the County's 50% contribution.
<b>Project Assumptions</b>	The project assumes that a Class A Burn Permit for the Sharpsburg Ave. CPSTC will not be obtained.
<b>Project Justification</b>	Due to the lack of foreseeable CIP funds for the new PSTC on Sharpsburg Pike, investing in a joint cost and use sharing venture with the City of Hagerstown creates a path forward to maintain needed live fire training at a facility that can meet current training needs for the City and County emergency responders. The current class A burn building on Bowman Avenue cannot continue to meet those needs due to its age and structural limitations.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	500,000	0	0	500,000	0	0	0	0	0
<b>Total Cost</b>	<b>500,000</b>	<b>0</b>	<b>0</b>	<b>500,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
General Fund	500,000	0	0	500,000	0	0	0	0	0
<b>Total Funding</b>	<b>500,000</b>	<b>0</b>	<b>0</b>	<b>500,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

**Project Title**                      **P25 UHF Public Safety Radio Communications System Upgrade**

**Full-time Employees**            0

**Operating Costs**                \$0

**Project Description**            P25 UHF Public Safety Radio Communications System Upgrade focuses on the upgrade of the system's core components to enable IP simulcast and P25 Phase 2 compatibility. Payments are stretched over ten (10) years.

**Project Assumptions** P25 UHF Public Safety Radio Communication System provides Washington County's first responders (Sheriff's Office, Emergency Services and multiple local municipal police agencies) with an IP simulcast and P25 Phase 2 compliant interoperable radio communications system.

**Project Justification**           The goal of the P25 UHF Public Safety Radio Communications System project is to either upgrade the current Motorola system while adding more proprietary system restrictions or opting to replace the current system with a P25 Phase 2 compatible open system platform for the same cost. FY 2021 costs include costs for optional consultant fees to assist the County in making a final determination on which direction it should proceed.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Hardware/Software	5,600,000	0	0	0	700,000	700,000	700,000	700,000	2,800,000
Other	200,000	0	0	200,000	0	0	0	0	0
<b>Total Cost</b>	<b>5,800,000</b>	<b>0</b>	<b>0</b>	<b>200,000</b>	<b>700,000</b>	<b>700,000</b>	<b>700,000</b>	<b>700,000</b>	<b>2,800,000</b>
<b><u>Funding Sources:</u></b>									
General Fund	5,800,000	0	0	200,000	700,000	700,000	700,000	700,000	2,800,000
<b>Total Funding</b>	<b>5,800,000</b>	<b>0</b>	<b>0</b>	<b>200,000</b>	<b>700,000</b>	<b>700,000</b>	<b>700,000</b>	<b>700,000</b>	<b>2,800,000</b>



Project Title Patrol Services Relocation

Full-time Employees 0

Operating Costs \$0

Project Description Relocation of Patrol Services to the County's Phoenix Color Building.

Project Justification The Patrol Division of the Sheriff's Office has outgrown the current facility due to the addition of personnel and positions within the division. All useable space is currently occupied. This project is needed in the next couple of years and will renovate 34,000 square feet of the warehouse portion of the Phoenix Color Building to house the Patrol Division. The current Patrol Division Building would then be used to house the Detention Center Administration, Detention Training and Detention Programs Unit. This would then allow for the expansion of Female Housing in the Detention Center, which is needed to house this growing population in the jail.

Project Costs:

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
Architect Fees	500,000	0	0	0	0	500,000	0	0	0
Construction	4,580,000	0	0	0	0	0	500,000	500,000	3,580,000
Inspection	100,000	0	0	0	0	0	0	0	100,000
Total Cost	5,180,000	0	0	0	0	500,000	500,000	500,000	3,680,000

Funding Sources:

General Fund	3,500,000	0	0	0	0	500,000	500,000	500,000	2,000,000
Tax-Supported Bond	1,680,000	0	0	0	0	0	0	0	1,680,000
Total Funding	5,180,000	0	0	0	0	500,000	500,000	500,000	3,680,000

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# Railroad Crossings

*Railroad Crossings*  
*Capital Improvement Ten Year Summary*  
*Fiscal Year 2021 - 2030*

Page	Project	Total	Prior Appr.	Budget Year		Ten Year Capital Program					Future
				FY 2021	FY 2022	FY 2023	FY 2024	FY 2025	FY 2026		
Project Costs											
<u>Railroad Crossings</u>											
170	Railroad Crossing Improvements	2,044,837	669,837	0	295,000	0	0	348,000	0	732,000	
Railroad Crossings Total		2,044,837	669,837	0	295,000	0	0	348,000	0	732,000	
Funding Sources											
General Fund		2,044,837	669,837	0	295,000	0	0	348,000	0	732,000	
		2,044,837	669,837	0	295,000	0	0	348,000	0	732,000	

Project Title	Railroad Crossing Improvements
Project ID	RRX003
Full-time Employees	0
Operating Costs	\$0

Project Description	The project will provide crossing improvements based on a priority ranking of all crossings in the County. Improvements will include flattening the approach roadway grades and improving the alignment, installing signs and pavement markings, improving sight distance by removing trees/brush/rock outcroppings, and utility relocations.
Project Assumptions	The railroad study utilizes methodologies set forth in the Railroad Highway Grade Crossing Handbook published by the Federal Railroad Administration (FRA). In addition to analyzing each intersection for potential collision and sight distance hazards, other factors such as accident records, development potential in the area surrounding the crossing, number of school buses, and the overall condition of the crossing and the surrounding pavement will be taken into account in assessing the need for improvements to the crossing. The first phase of the work will involve installing passive warning devices (signs and pavement markings) to be in compliance with the Manual on Uniform Traffic Control Devices (MUTCD) and FRA. The second phase of work will involve providing road improvements. Some crossings may require installation of active warning devices (flashing lights, automatic gates, etc.). These improvements will be coordinated with the railroad companies.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	1,910,837	535,837	0	295,000	0	0	348,000	0	732,000
Land Acquisition	112,000	112,000	0	0	0	0	0	0	0
Utilities	22,000	22,000	0	0	0	0	0	0	0
Total Cost	2,044,837	669,837	0	295,000	0	0	348,000	0	732,000
<b>Funding Sources:</b>									
General Fund	2,044,837	669,837	0	295,000	0	0	348,000	0	732,000
Total Funding	2,044,837	669,837	0	295,000	0	0	348,000	0	732,000

# Road Improvements

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*Road Improvement  
Capital Improvement Ten Year Summary  
Fiscal Year 2021 – 2030*

Page	Project	Budget Year				Ten Year Capital Program				
		Total	Prior Appr.	FY 2021	FY 2022	FY 2023	FY 2024	FY 2025	FY 2026	Future
Project Costs										
Road Improvement										
172	Transportation ADA	1,195,450	351,450	82,000	83,000	84,000	86,000	87,000	84,000	338,000
173	Pavement Maintenance and Rehab Program	60,818,787	8,822,787	4,246,000	5,000,000	5,250,000	5,250,000	5,250,000	5,250,000	21,750,000
174	Longmeadow Road	810,000	0	0	0	0	0	310,000	500,000	0
175	Eastern Boulevard Extended	7,850,000	0	0	0	0	1,347,000	804,000	1,556,000	4,143,000
176	Eastern Boulevard Widening	5,672,300	2,691,300	2,081,000	500,000	400,000	0	0	0	0
177	Eastern Blvd at Antietam Drive Improvements	2,506,000	2,006,000	0	0	500,000	0	0	0	0
178	Professional Boulevard Extended Phase II	4,309,200	4,059,200	250,000	0	0	0	0	0	0
179	Valley Mall Area Road Improvements Phase II	942,000	200,000	0	0	0	142,000	600,000	0	0
180	Professional Boulevard Extended Phase III	1,203,000	0	0	0	121,000	1,082,000	0	0	0
181	Showalter Road Extended East	2,983,000	510,000	0	0	0	0	0	0	2,473,000
182	Halfway Boulevard Extended	5,973,000	3,073,000	2,000,000	400,000	500,000	0	0	0	0
183	Wright Road	2,750,000	0	454,000	1,268,000	528,000	500,000	0	0	0
184	Burnside Bridge Road Spot Improvements	544,000	0	0	0	0	0	0	0	544,000
185	Colonel Henry Douglas Drive Extended Phase II	400,000	0	0	0	0	0	0	0	400,000
186	E. Oak Ridge Drive/South Pointe Signal	461,000	0	0	0	0	0	111,000	350,000	0
187	Mt Aetna Road Spot	1,727,000	0	0	0	0	0	0	0	1,727,000
188	Professional Boulevard Extended - Phase IV	800,000	0	0	0	0	0	800,000	0	0
189	Rockdale Road and Independence Road Spot Improvements	1,025,000	0	0	0	0	0	0	0	1,025,000
190	Highway - Vehicle & Equipment Replacement Program	14,883,122	1,135,122	1,068,000	1,000,000	1,084,000	1,322,000	1,389,000	1,458,000	6,427,000
191	Highway Maintenance Shop - Western Section	374,000	0	0	199,000	175,000	0	0	0	0
192	Salt Storage Structure	350,000	0	350,000	0	0	0	0	0	0
193	Highway Western Section - Fuel Tank Replacement	847,000	486,000	0	0	0	193,000	168,000	0	0
Road Improvement Total		118,423,859	23,334,859	10,531,000	8,450,000	8,642,000	9,922,000	9,519,000	9,198,000	38,827,000
Funding Sources										
General Fund		13,269,624	3,242,624	0	832,000	843,000	1,101,000	1,144,000	1,042,000	5,065,000
Highway Fund		6,111,027	1,111,027	500,000	500,000	500,000	500,000	500,000	500,000	2,000,000
Tax-Supported Bond		69,654,500	10,100,500	5,367,000	5,651,000	5,682,000	7,246,000	6,111,000	5,851,000	23,646,000
Transfer Tax		18,433,708	3,875,708	1,809,000	12,000	1,412,000	870,000	1,559,000	1,600,000	7,296,000
Excise Tax - Roads		1,530,320	270,320	126,000	126,000	126,000	126,000	126,000	126,000	504,000
Excise Tax - Other		319,000	29,000	29,000	29,000	29,000	29,000	29,000	29,000	116,000
Excise Tax - Non-Residential		550,000	50,000	50,000	50,000	50,000	50,000	50,000	50,000	200,000
APFO Fees - Roads		300,000	300,000	0	0	0	0	0	0	0
Capital Reserve - General		650,000	0	650,000	0	0	0	0	0	0
Federal Grant		6,155,680	3,155,680	2,000,000	1,000,000	0	0	0	0	0
State Grant		1,200,000	1,200,000	0	0	0	0	0	0	0
Contributions		250,000	0	0	250,000	0	0	0	0	0
		118,423,859	23,334,859	10,531,000	8,450,000	8,642,000	9,922,000	9,519,000	9,198,000	38,827,000

Project Title	Transportation ADA
Project ID	LDI037
Full-time Employees	0
Operating Costs	\$0

Project Description	This project will provide upgrades to existing street crossings and facilities to comply with Americans with Disabilities (ADA) requirements. This includes providing pavement markings, signs, sidewalks, ramps, and accessible pedestrian signals as necessary to bring the existing conditions into compliance.
Project Assumptions	The project consists of a multi-year plan to achieve full compliance. The project received an APFO contribution for improvements in Maugansville.
Project Justification	State and local governments must maintain accessible features in operable working condition to comply with Title II of ADA. At a minimum, government entities need to comply with certain administrative requirements, conduct a self-evaluation, involve the public, develop/implement a transition plan, and provide accessibility during construction. FHWA is now requiring sub-recipient entities to have and implement a transition plan to bring the transportation system into full compliance.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	1,195,450	351,450	82,000	83,000	84,000	86,000	87,000	84,000	338,000
Total Cost	1,195,450	351,450	82,000	83,000	84,000	86,000	87,000	84,000	338,000
<b>Funding Sources:</b>									
General Fund	1,113,450	351,450	0	83,000	84,000	86,000	87,000	84,000	338,000
Capital Reserve - General	82,000	0	82,000	0	0	0	0	0	0
Total Funding	1,195,450	351,450	82,000	83,000	84,000	86,000	87,000	84,000	338,000



Project Title	Pavement Maintenance and Rehab
Program Project ID	RDI024
Full-time Employees	0
Operating Costs	\$0

Project Description	This project includes the modification of county-wide pavement maintenance program targeting rehabilitation of county highway pavement, as required. Techniques may include but not be limited to road reclamation, bituminous concrete overlay, crack sealing, and surface treatment. Individual projects will be determined on an annual basis consistent with the County's overall Pavement Management Program.
Project Assumptions	Financial and engineering analysis has determined that at least an \$8M per year investment is necessary to keep up with pavement deterioration.
Project Justification	The project is needed to launch an aggressive overall highway pavement rehabilitation program to address major pavement deficiencies throughout our existing highway system. This program will result in longer service life of our pavements and improved service using the most cost-effective treatment at the appropriate time.

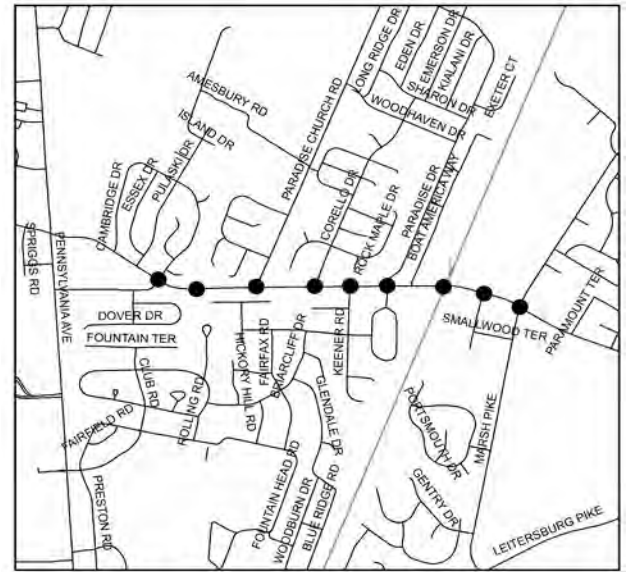
	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	58,043,787	8,547,787	3,996,000	4,750,000	5,000,000	5,000,000	5,000,000	5,000,000	20,750,000
Inspection	2,775,000	275,000	250,000	250,000	250,000	250,000	250,000	250,000	1,000,000
<b>Total Cost</b>	<b>60,818,787</b>	<b>8,822,787</b>	<b>4,246,000</b>	<b>5,000,000</b>	<b>5,250,000</b>	<b>5,250,000</b>	<b>5,250,000</b>	<b>5,250,000</b>	<b>21,750,000</b>
<b>Funding Sources:</b>									
General Fund	2,197,079	2,197,079	0	0	0	0	0	0	0
Highway Fund	350,000	0	0	50,000	0	0	0	0	300,000
Tax-Supported Bond	37,583,000	2,545,000	2,232,000	4,733,000	3,633,000	4,175,000	3,486,000	3,445,000	13,334,000
Transfer Tax	18,433,708	3,875,708	1,809,000	12,000	1,412,000	870,000	1,559,000	1,600,000	7,296,000
Excise Tax - Roads	1,386,000	126,000	126,000	126,000	126,000	126,000	126,000	126,000	504,000
Excise Tax - Other	319,000	29,000	29,000	29,000	29,000	29,000	29,000	29,000	116,000
Excise Tax - Non-Residential	550,000	50,000	50,000	50,000	50,000	50,000	50,000	50,000	200,000
<b>Total Funding</b>	<b>60,818,787</b>	<b>8,822,787</b>	<b>4,246,000</b>	<b>5,000,000</b>	<b>5,250,000</b>	<b>5,250,000</b>	<b>5,250,000</b>	<b>5,250,000</b>	<b>21,750,000</b>

**Project Title** Longmeadow Road

**Project ID** RDI037

**Full-time Employees** 0

**Operating Costs** \$4,000



**Project Description** The project will provide capacity and safety enhancement to Longmeadow Road between Halifax Drive and Marsh Pike. The project includes widening Longmeadow Road to a three-lane roadway and the construction of a traffic signal at the intersection with Paradise Church Road and/or Halifax Drive if warranted. The roadway will be a combination of open and closed section roadway with one lane in each direction and a continuous center left turn lane.

**Project Assumptions** County funding is shown; however, private developer contributions will be sought for the signals if a private developer project is initiated in the area.

**Project Justification** Current traffic volume on Longmeadow Road exceeds 12,000 vehicles per day and is anticipated to continue to climb based on development. The roadway and intersections operate at unacceptable levels of service.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	710,000	0	0	0	0	0	210,000	500,000	0
Engineering and Design	100,000	0	0	0	0	0	100,000	0	0
<b>Total Cost</b>	<b>810,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>310,000</b>	<b>500,000</b>	<b>0</b>
<b>Funding Sources:</b>									
Tax-Supported Bond	810,000	0	0	0	0	0	310,000	500,000	0
<b>Total Funding</b>	<b>810,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>310,000</b>	<b>500,000</b>	<b>0</b>

Project Title	Eastern Boulevard Extended
Project ID	RDI039
Full-time Employees	0
Operating Costs	\$4,000



Project Description	The project involves the construction of a new roadway connecting Eastern Boulevard at Antietam Drive and Leithersburg Pike at Marsh Pike. The road is approximately 6,000' in length and will connect with the new intersection at Antietam Drive and involve intersection improvements at MD 60/Marsh Pike that will include construction of a traffic signal or roundabout. The project also includes construction of a major drainage culvert.
Project Assumptions	The majority of the right-of-way is donated. The project assumes remaining lands necessary for completion can be obtained within budgeted values. The project involves construction of a three-lane roadway (one lane in each direction with a continuous center left turn lane). SHA may require significant road widening along MD 60.
Project Justification	The project will relieve Eastern Blvd at Leithersburg Pike intersection congestion. The project provides significant enhancement to the transportation network increasing capacity, improving safety and providing access to developable land including a parcel planned for a future County park.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	7,350,000	0	0	0	0	997,000	654,000	1,556,000	4,143,000
Engineering and Design	50,000	0	0	0	0	50,000	0	0	0
Inspection	150,000	0	0	0	0	0	150,000	0	0
Land Acquisition	150,000	0	0	0	0	150,000	0	0	0
Utilities	150,000	0	0	0	0	150,000	0	0	0
Total Cost	7,850,000	0	0	0	0	1,347,000	804,000	1,556,000	4,143,000
<b>Funding Sources:</b>									
Tax-Supported Bond	7,850,000	0	0	0	0	1,347,000	804,000	1,556,000	4,143,000
Total Funding	7,850,000	0	0	0	0	1,347,000	804,000	1,556,000	4,143,000

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Construction	4,686,000	1,855,000	1,931,000	500,000	400,000	0	0	0	0
Engineering and Design	500,000	500,000	0	0	0	0	0	0	0
Inspection	150,000	0	150,000	0	0	0	0	0	0
Land Acquisition	336,300	336,300	0	0	0	0	0	0	0
<b>Total Cost</b>	<b>5,672,300</b>	<b>2,691,300</b>	<b>2,081,000</b>	<b>500,000</b>	<b>400,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
General Fund	195,000	195,000	0	0	0	0	0	0	0
Tax-Supported Bond	4,977,300	1,996,300	2,081,000	500,000	400,000	0	0	0	0
Excise Tax - Roads	144,320	144,320	0	0	0	0	0	0	0
Federal Grant	355,680	355,680	0	0	0	0	0	0	0
<b>Total Funding</b>	<b>5,672,300</b>	<b>2,691,300</b>	<b>2,081,000</b>	<b>500,000</b>	<b>400,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

**Project Title** Eastern Blvd at Antietam Drive Improvements

**Project ID** RDI051

**Full-time Employees** 0

**Operating Costs** \$0

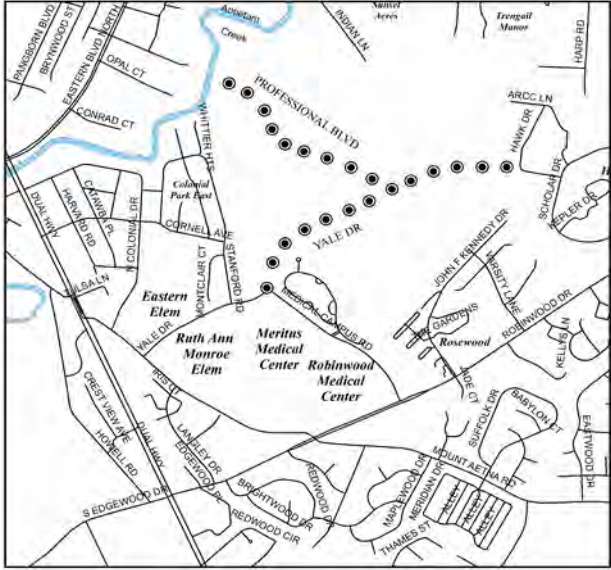


**Project Description** The project will provide intersection improvements at Antietam Drive. This project will connect the Eastern Boulevard Widening Phase II project with the Eastern Boulevard Extended project.

**Project Justification** Existing traffic volumes on Eastern Boulevard exceed capacity and land development activity is anticipated in the area. Regional growth in the greater Hagerstown area will cause traffic volumes to increase. Advancing this intersection project will enhance traffic safety and reduce travel speeds in the area.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	1,882,000	1,382,000	0	0	500,000	0	0	0	0
Engineering and Design	300,000	300,000	0	0	0	0	0	0	0
Inspection	122,000	122,000	0	0	0	0	0	0	0
Land Acquisition	202,000	202,000	0	0	0	0	0	0	0
<b>Total Cost</b>	<b>2,506,000</b>	<b>2,006,000</b>	<b>0</b>	<b>0</b>	<b>500,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>Funding Sources:</b>									
General Fund	2,000	2,000	0	0	0	0	0	0	0
Tax-Supported Bond	2,504,000	2,004,000	0	0	500,000	0	0	0	0
<b>Total Funding</b>	<b>2,506,000</b>	<b>2,006,000</b>	<b>0</b>	<b>0</b>	<b>500,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

Project Title	Professional Boulevard Extended - Phase II
Project ID	RDI056
Full-time Employees	0
Operating Costs	\$3,000



Project Description	The project involves the extension of Professional Court from a point 200 feet east of the proposed Antietam Creek Bridge (Project 1072) to Yale Drive Extended (Project 1093). The roadway length is approximately 2,700 feet. The project includes construction of a four-lane closed section divided roadway that will connect into the roundabout intersection at Yale Drive.
Project Assumptions	No new water and/or sewer network is included in these costs and must be funded from other sources (other project or private sector investment). The County is currently seeking a state/federal grant to assist in covering construction costs. Includes ARC grant of \$1M for a portion of Phase II, referred to as Phase II-A. Also includes \$1.2M in State Aid for Phase II.
Project Justification	Regional traffic is anticipated to significantly increase requiring more transportation connectivity in this area. The project area has been identified as an economic development opportunity for the County.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	3,630,000	3,380,000	250,000	0	0	0	0	0	0
Engineering and Design	312,200	312,200	0	0	0	0	0	0	0
Inspection	265,000	265,000	0	0	0	0	0	0	0
Land Acquisition	102,000	102,000	0	0	0	0	0	0	0
Total Cost	4,309,200	4,059,200	250,000	0	0	0	0	0	0
<b>Funding Sources:</b>									
General Fund	89,000	89,000	0	0	0	0	0	0	0
Tax-Supported Bond	2,020,200	1,770,200	250,000	0	0	0	0	0	0
Federal Grant	1,000,000	1,000,000	0	0	0	0	0	0	0
State Grant	1,200,000	1,200,000	0	0	0	0	0	0	0
Total Funding	4,309,200	4,059,200	250,000	0	0	0	0	0	0

Project Title

Valley Mall Area Road Improvements Phase II

Project ID

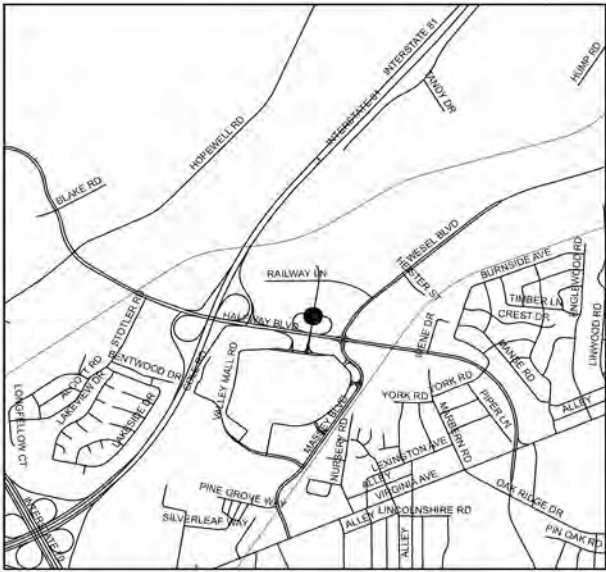
RDI062

Full-time Employees

0

Operating Costs

\$0



Project Description

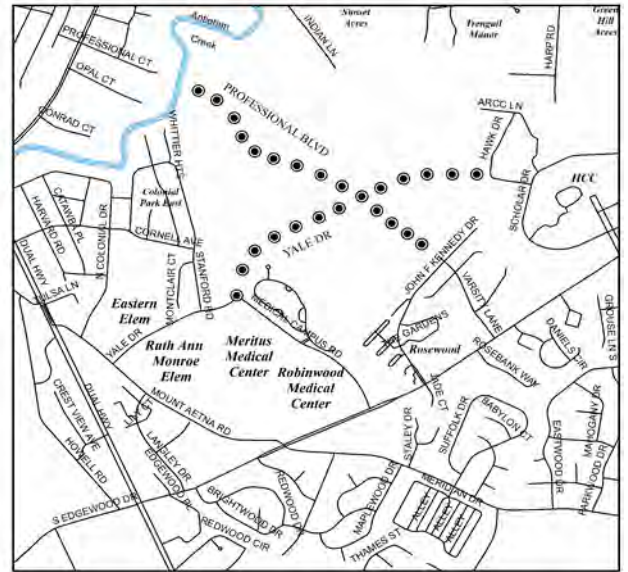
This project includes intersection reconstruction on Underpass Way at the on/off ramps to/from Halfway Blvd. The project will convert a stop-controlled intersection to a roundabout intersection.

Project Justification

Traffic volume increases in this area are making turning movements at this location more difficult. Once volumes warrant the signal or roundabout, the safety and traffic flow will be improved.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
Project Costs:									
Construction	942,000	200,000	0	0	0	142,000	600,000	0	0
Total Cost	942,000	200,000	0	0	0	142,000	600,000	0	0
Funding Sources:									
Tax-Supported Bond	942,000	200,000	0	0	0	142,000	600,000	0	0
Total Funding	942,000	200,000	0	0	0	142,000	600,000	0	0

<b>Project Title</b>	<b>Professional Boulevard Extended - Phase III</b>
<b>Project ID</b>	RDI064
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$1,000

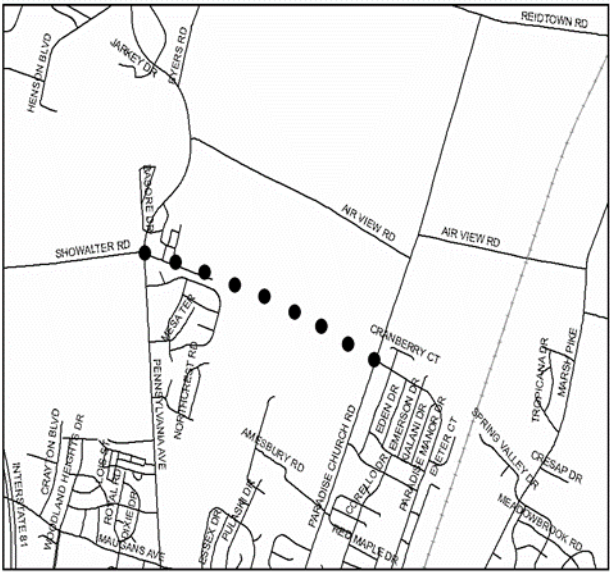


<b>Project Description</b>	The project involves the construction of a segment of Professional Boulevard south from the second Yale Drive roundabout continuing southeast towards Phase IV and Robinwood Drive. The roadway length is approximately 900 feet. The project includes construction of a four-lane closed section roadway.
<b>Project Assumptions</b>	No new water and/or sewer network is included in these costs and must be funded from other sources (other project or private sector investment).
<b>Project Justification</b>	Regional traffic is anticipated to significantly increase requiring more transportation connectivity in this area. The project area has been identified as an economic development opportunity for the County.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	1,103,000	0	0	0	121,000	982,000	0	0	0
Inspection	100,000	0	0	0	0	100,000	0	0	0
<b>Total Cost</b>	<b>1,203,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>121,000</b>	<b>1,082,000</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
Tax-Supported Bond	1,203,000	0	0	0	121,000	1,082,000	0	0	0
<b>Total Funding</b>	<b>1,203,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>121,000</b>	<b>1,082,000</b>	<b>0</b>	<b>0</b>	<b>0</b>



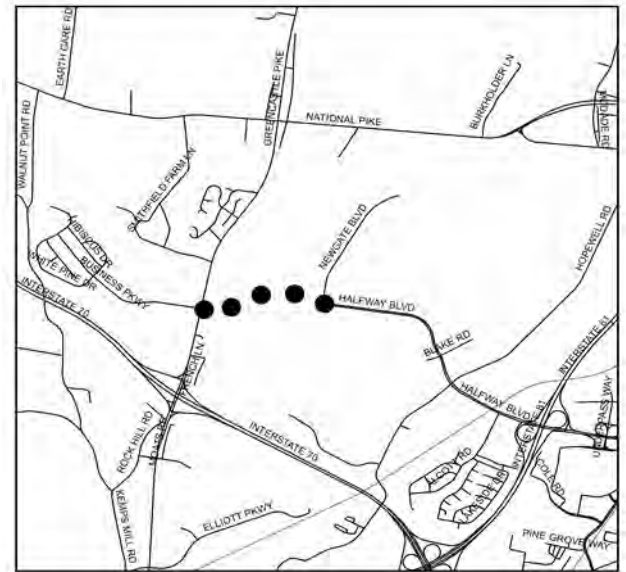
Project Title	Showalter Road Extended East
Project ID	RDI068
Full-time Employees	0
Operating Costs	\$4,000



Project Description	The project involves the extension of Showalter Road from US 11 to Paradise Church Road. The proposed road length is approximately 5,800 LF. Intersection improvements are included at each end of the project.
Project Assumptions	The project involves construction of a two-lane roadway. No new water and/or sewer network is included in this cost estimate.
Project Justification	The project is needed to accommodate increased traffic volumes from ongoing development in this area.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	1,742,000	0	0	0	0	0	0	0	1,742,000
Engineering and Design	223,000	102,000	0	0	0	0	0	0	121,000
Land Acquisition	1,018,000	408,000	0	0	0	0	0	0	610,000
Total Cost	2,983,000	510,000	0	0	0	0	0	0	2,473,000
<b>Funding Sources:</b>									
Tax-Supported Bond	2,983,000	510,000	0	0	0	0	0	0	2,473,000
Total Funding	2,983,000	510,000	0	0	0	0	0	0	2,473,000

<b>Project Title</b>	<b>Halfway Boulevard Extended Phase I</b>
<b>Project ID</b>	RDI069
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$2,000



<b>Project Description</b>	The project involves the construction of a new four-lane open section roadway, including a large culvert, connecting existing Halfway Boulevard to MD Route 63. Intersection improvements at Halfway Boulevard extended and MD Route 63 include a traffic signal.
<b>Project Assumptions</b>	ARC grants totaling \$3,800,000 are being secured for this project. This project is receiving developer contributions for improvements.
<b>Project Justification</b>	Regional traffic is anticipated to significantly increase, requiring more transportation connectivity in this area. The project provides an alternate access route to MD Route 63, Interstate 70 and Interstate 81 for heavy truck traffic from the industrial park and businesses on Halfway Boulevard. The project area has been identified as an economic development opportunity for the County.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	5,623,000	2,973,000	2,000,000	400,000	250,000	0	0	0	0
Engineering and Design	100,000	100,000	0	0	0	0	0	0	0
Inspection	250,000	0	0	0	250,000	0	0	0	0
<b>Total Cost</b>	<b>5,973,000</b>	<b>3,073,000</b>	<b>2,000,000</b>	<b>400,000</b>	<b>500,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
Tax-Supported Bond	1,873,000	973,000	0	400,000	500,000	0	0	0	0
APFO Fees - Roads	300,000	300,000	0	0	0	0	0	0	0
Federal Grant	3,800,000	1,800,000	2,000,000	0	0	0	0	0	0
<b>Total Funding</b>	<b>5,973,000</b>	<b>3,073,000</b>	<b>2,000,000</b>	<b>400,000</b>	<b>500,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

Project Title

Wright Road

Project ID

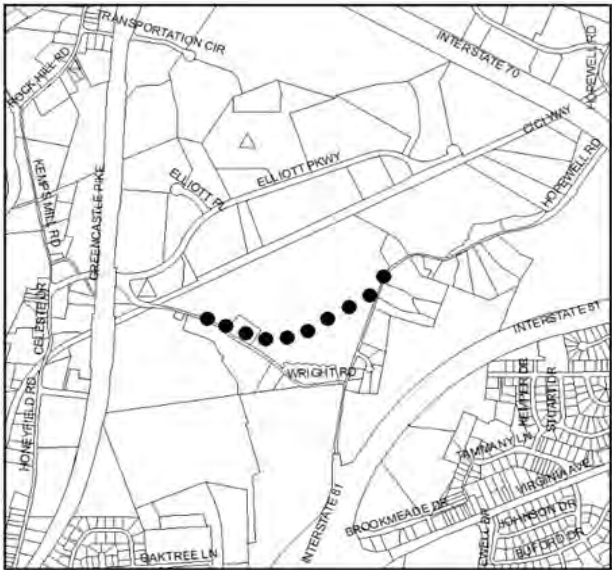
RDI070

Full-time Employees

0

Operating Costs

\$1,080



Project Description

The project involves the relocation of Wright Road between Hopewell Road and the railroad crossing on Wright Road. The project includes construction of a three-lane closed section (one lane in each direction with a continuous left turn lane) and will include removal of the one lane culvert in the floodplain on Wright Road at the intersection of Hopewell Road. The project length is approximately 2,000 LF.

Project Assumptions

No new water and/or sewer network is included in these costs and must be funded from other sources (other projects or private sector investment). An application for an ARC grant in the amount of \$1,000,000 will be submitted. This project assumes \$250,000 in private development contributions. Project is dependent upon receiving ARC funding and developer contributions.

Project Justification

Wright Road in this area is currently in the floodplain, and the frequency of flooding impacts motorists and the delivery of public safety services in that area of the County. The project will accommodate increased traffic volumes from development in this area and has been identified as an economic development opportunity for the County.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	2,500,000	0	354,000	1,268,000	528,000	350,000	0	0	0
Engineering and Design	100,000	0	100,000	0	0	0	0	0	0
Inspection	150,000	0	0	0	0	150,000	0	0	0
Total Cost	2,750,000	0	454,000	1,268,000	528,000	500,000	0	0	0
<b>Funding Sources:</b>									
Tax-Supported Bond	1,500,000	0	454,000	18,000	528,000	500,000	0	0	0
Federal Grant	1,000,000	0	0	1,000,000	0	0	0	0	0
Contributions	250,000	0	0	250,000	0	0	0	0	0
Total Funding	2,750,000	0	454,000	1,268,000	528,000	500,000	0	0	0

Project Title

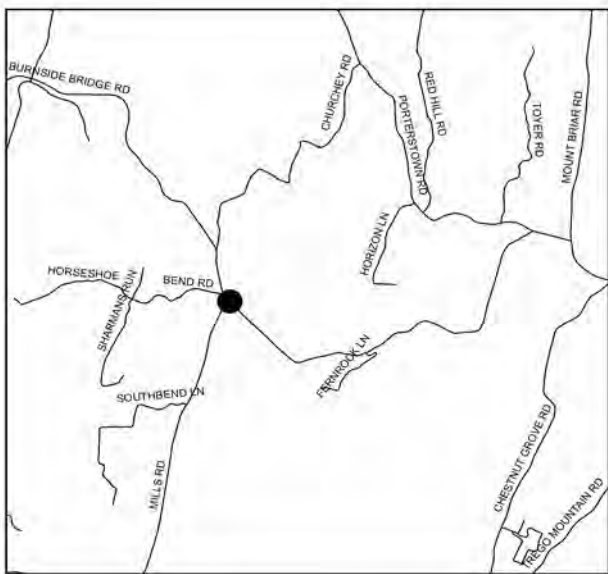
Burnside Bridge Road Spot Improvements

Full-time Employees

0

Operating Costs

\$0



Project Description

This project will provide for spot road improvements to Burnside Bridge Road at the intersection with Mills Road. The project includes correcting vertical curves and horizontal curves (blind hills).

Project Justification

Burnside Bridge Road at Mills Road intersection has been identified as inadequate as defined in the Road Adequacy Policy.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	295,000	0	0	0	0	0	0	0	295,000
Engineering and Design	89,000	0	0	0	0	0	0	0	89,000
Inspection	71,000	0	0	0	0	0	0	0	71,000
Land Acquisition	30,000	0	0	0	0	0	0	0	30,000
Utilities	59,000	0	0	0	0	0	0	0	59,000
Total Cost	544,000	0	0	0	0	0	0	0	544,000
<b>Funding Sources:</b>									
Tax-Supported Bond	544,000	0	0	0	0	0	0	0	544,000
Total Funding	544,000	0	0	0	0	0	0	0	544,000

Project Title

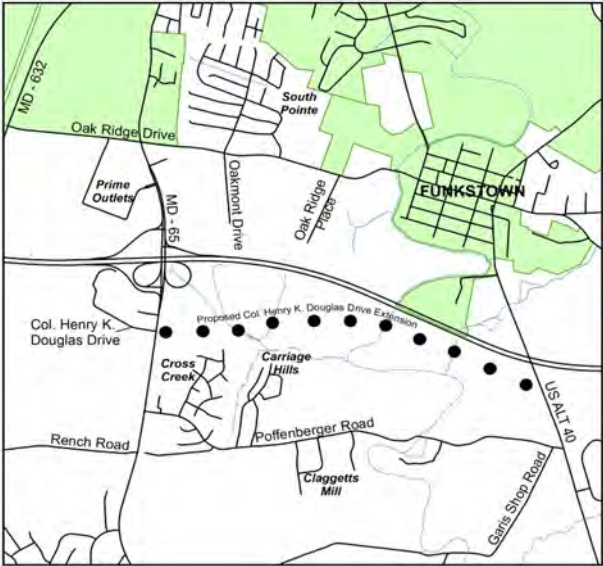
Colonel Henry Douglas Drive Extended Phase II

Full-time Employees

0

Operating Costs

\$3,000



Project Description

The project involves the extension of Colonel Henry K Douglas Drive from the new bridge over the Antietam Creek to Alternate 40 (ADC Map 21, F-13). The roadway is approximately 3/4-mile-long and will include construction of a two-lane closed section roadway.

Project Assumptions

No new water and/or sewer network is included in these costs and must be funded from other sources (other project or private sector investment).

Project Justification

Regional traffic is anticipated to significantly increase requiring more transportation connectivity in this area. The project area has been identified as an economic opportunity for the County.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Land Acquisition	400,000	0	0	0	0	0	0	0	400,000
<b>Total Cost</b>	<b>400,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>400,000</b>
<b>Funding Sources:</b>									
Tax-Supported Bond	400,000	0	0	0	0	0	0	0	400,000
<b>Total Funding</b>	<b>400,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>400,000</b>

Project Title	E. Oak Ridge Drive/South Pointe Signal
Full-time Employees	0
Operating Costs	\$1,000



**Project Description** This project will construct a traffic signal at the intersection of E. Oak Ridge Drive and South Pointe Drive/Oakmont Drive.

**Project Justification** Traffic volume increases in this area are making turning movements at this location more difficult and potentially unsafe. Once volumes warrant the signal, safety and traffic flow will be improved.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	300,000	0	0	0	0	0	0	300,000	0
Engineering and Design	37,000	0	0	0	0	0	37,000	0	0
Inspection	50,000	0	0	0	0	0	0	50,000	0
Land Acquisition	21,000	0	0	0	0	0	21,000	0	0
Utilities	53,000	0	0	0	0	0	53,000	0	0
<b>Total Cost</b>	<b>461,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>111,000</b>	<b>350,000</b>	<b>0</b>
<b>Funding Sources:</b>									
Tax-Supported Bond	461,000	0	0	0	0	0	111,000	350,000	0
<b>Total Funding</b>	<b>461,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>111,000</b>	<b>350,000</b>	<b>0</b>

Project Title

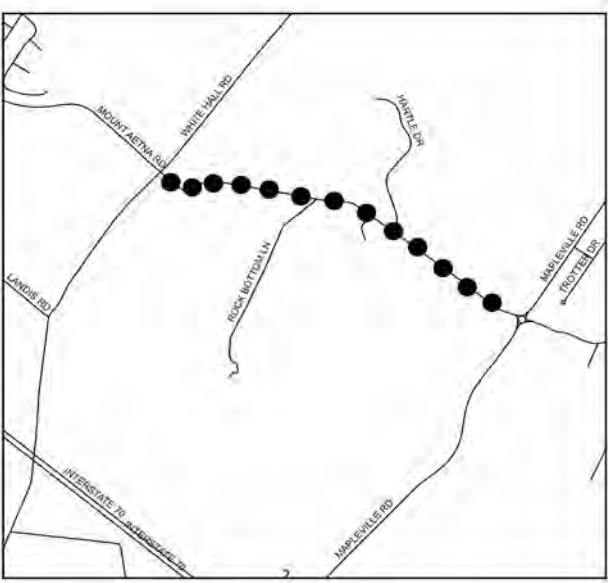
Mt Aetna Road Spot Improvements

Full-time Employees

0

Operating Costs

\$3,000



Project Description

The project includes providing spot improvements to Mt Aetna Road to improve sight distance, and vertical/horizontal curves.

Project Justification

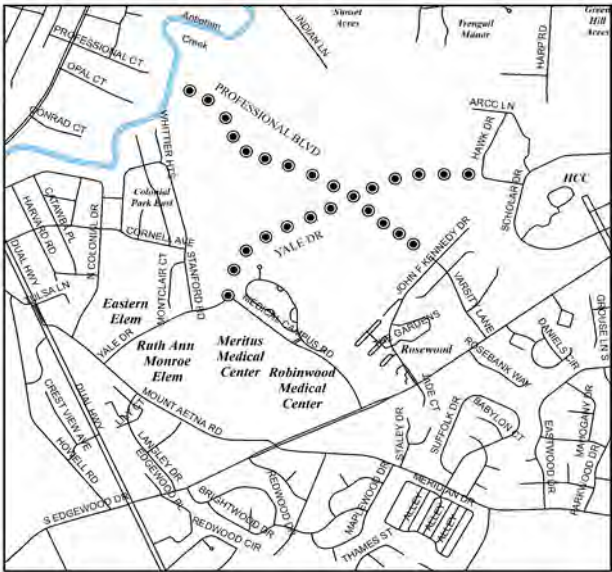
Mt Aetna Road has several locations that have been identified as inadequate as defined in the "Road Adequacy Policy". Development in the vicinity has caused an increase in traffic.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	1,597,000	0	0	0	0	0	0	0	1,597,000
Inspection	71,000	0	0	0	0	0	0	0	71,000
Land Acquisition	59,000	0	0	0	0	0	0	0	59,000
Total Cost	1,727,000	0	0	0	0	0	0	0	1,727,000
<b>Funding Sources:</b>									
Tax-Supported Bond	1,727,000	0	0	0	0	0	0	0	1,727,000
Total Funding	1,727,000	0	0	0	0	0	0	0	1,727,000

**Project Title** Professional Boulevard Extended - Phase IV

**Full-time Employees** 0

**Operating Costs** \$1,000



**Project Description** The project involves the construction of a segment of Professional Boulevard south of Yale Drive continuing from Phase III southeast towards Robinwood Drive. The roadway length is approximately 900 feet. The project includes construction of a four-lane closed section roadway.

**Project Assumptions** No new water and/or sewer network is included in these costs and must be funded from other sources (other project or private sector investment).

**Project Justification** Regional traffic is anticipated to significantly increase requiring more transportation connectivity in this area. The project area has been identified as an economic development opportunity for the County.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	750,000	0	0	0	0	0	750,000	0	0
Inspection	50,000	0	0	0	0	0	50,000	0	0
<b>Total Cost</b>	<b>800,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>800,000</b>	<b>0</b>	<b>0</b>
<b>Funding Sources:</b>									
Tax-Supported Bond	800,000	0	0	0	0	0	800,000	0	0
<b>Total Funding</b>	<b>800,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>800,000</b>	<b>0</b>	<b>0</b>



Project Title Improvements

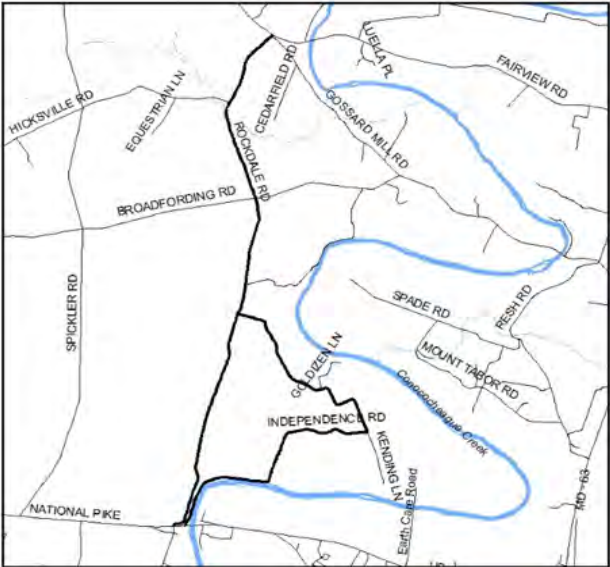
Rockdale Road and Independence Road Spot

Full-time Employees

0

Operating Costs

\$3,600



Project Description

The project will provide for spot road improvements on Rockdale Road and Independence Road.

Project Justification

Portions of Rockdale Road and Independence Road have been identified as inadequate as defined in the Road Adequacy Policy.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	800,000	0	0	0	0	0	0	0	800,000
Inspection	75,000	0	0	0	0	0	0	0	75,000
Land Acquisition	100,000	0	0	0	0	0	0	0	100,000
Utilities	50,000	0	0	0	0	0	0	0	50,000
Total Cost	1,025,000	0	0	0	0	0	0	0	1,025,000
<b>Funding Sources:</b>									
Tax-Supported Bond	1,025,000	0	0	0	0	0	0	0	1,025,000
Total Funding	1,025,000	0	0	0	0	0	0	0	1,025,000

Project Title	Highway - Equipment and Vehicle Replacement
Program Project ID	EQP042
Full-time Employees	0
Operating Costs	\$0

Project Description	This project will replace vehicles and heavy/specialized equipment for maintenance and construction activity.
Project Assumptions	Recommendation for replacement is based on a review of policy, equipment condition, and consideration of funding levels that exist.
Project Justification	The County maintains an inventory of specialized and heavy equipment for maintenance and construction activities. Each year, the Public Works Department analyzes the condition of this equipment to identify candidates for replacement. This analysis is conducted in conjunction with the user departments and focuses on factors such as age, mileage and/or hours, and life-to-date repair history.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Equipment/Furniture	14,883,122	1,135,122	1,068,000	1,000,000	1,084,000	1,322,000	1,389,000	1,458,000	6,427,000
<b>Total Cost</b>	<b>14,883,122</b>	<b>1,135,122</b>	<b>1,068,000</b>	<b>1,000,000</b>	<b>1,084,000</b>	<b>1,322,000</b>	<b>1,389,000</b>	<b>1,458,000</b>	<b>6,427,000</b>
<b>Funding Sources:</b>									
General Fund	9,291,095	400,095	0	550,000	584,000	1,015,000	1,057,000	958,000	4,727,000
Highway Fund	5,024,027	735,027	500,000	450,000	500,000	307,000	332,000	500,000	1,700,000
Capital Reserve - General	568,000	0	568,000	0	0	0	0	0	0
<b>Total Funding</b>	<b>14,883,122</b>	<b>1,135,122</b>	<b>1,068,000</b>	<b>1,000,000</b>	<b>1,084,000</b>	<b>1,322,000</b>	<b>1,389,000</b>	<b>1,458,000</b>	<b>6,427,000</b>

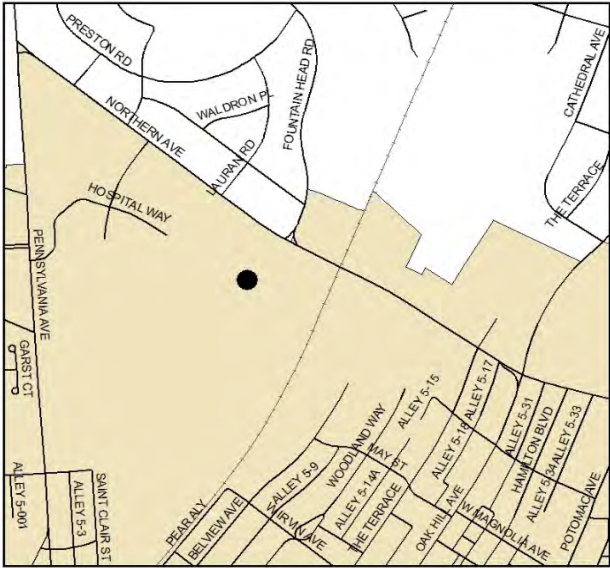
Project Title	Highway Maintenance Shop - Western Section
Project ID	BLD084
Full-time Employees	0
Operating Costs	\$3,000



Project Description	This project will provide for the construction of a 1,500 SF building addition to the garage area at the western section highway maintenance shop. Additional space is needed to perform maintenance on vehicles, which currently must occur outside (including during inclement weather).
Project Assumptions	The site is a highway maintenance facility in a remote area critical for winter weather operations and response to public emergency situations.
Project Justification	Highway maintenance is a core county service.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	337,000	0	0	162,000	175,000	0	0	0	0
Engineering and Design	37,000	0	0	37,000	0	0	0	0	0
Total Cost	374,000	0	0	199,000	175,000	0	0	0	0
<b>Funding Sources:</b>									
General Fund	374,000	0	0	199,000	175,000	0	0	0	0
Highway Fund	0	0	0	0	0	0	0	0	0
Tax-Supported Bond	0	0	0	0	0	0	0	0	0
Total Funding	374,000	0	0	199,000	175,000	0	0	0	0

Project Title	Salt Storage Structure
Project ID	BLD104
Full-time Employees	0
Operating Costs	\$0

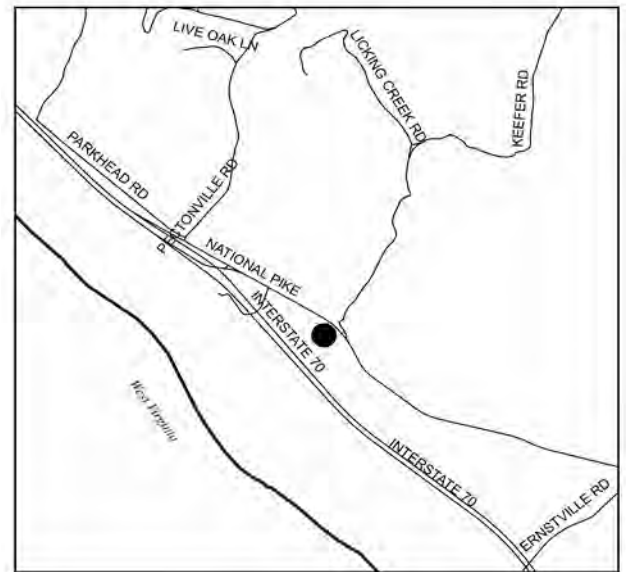


**Project Description** 107' wide X 100' long beam structure, covered w/29oz 30yr fabric

**Project Justification** To prevent run off into Hamilton run due to no current covered salt storage at Central Facility and to stop clumping and spreader jam.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	350,000	0	350,000	0	0	0	0	0	0
<b>Total Cost</b>	<b>350,000</b>	<b>0</b>	<b>350,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>Funding Sources:</b>									
Tax-Supported Bond	350,000	0	350,000	0	0	0	0	0	0
<b>Total Funding</b>	<b>350,000</b>	<b>0</b>	<b>350,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

<b>Project Title</b>	<b>HWY Western Section - Fuel Tank Replacement</b>
<b>Project ID</b>	EQP056
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$0



<b>Project Description</b>	The project will replace the existing diesel fuel center and add a gasoline fuel center at the western highway maintenance facility. The existing underground diesel storage tanks are old style steel tanks under impressed current cathodic protection. The addition of new gasoline tanks will provide 24 hour 7 days a week access for all county vehicles including Sheriff's Office and Board of Education vehicles operating in the western part of the County. The project includes modernizing the existing pump, addition of new pump, adding controlled access and integrated software systems to match existing fuel center systems used elsewhere in County government to provide connectivity to the central section highway shop software tracking system for billing and data collection. The project also includes a motorized entrance gate.
<b>Project Assumptions</b>	Tanks are 20+ years old and present an increased liability of an environmental release. This project will reduce operating expenses associated with fuel usage and vehicle maintenance.
<b>Project Justification</b>	This project will dramatically reduce the environmental threat of a leaking underground tank and pipes. This project will allow vehicles and employees operating in the western portion of the County, including law enforcement officers to remain in the area of service. By constructing this facility, these employees will not be taken out of their coverage area to refuel their vehicle.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	776,000	415,000	0	0	0	193,000	168,000	0	0
Engineering and Design	20,000	20,000	0	0	0	0	0	0	0
Inspection	51,000	51,000	0	0	0	0	0	0	0
<b>Total Cost</b>	<b>847,000</b>	<b>486,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>193,000</b>	<b>168,000</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
General Fund	8,000	8,000	0	0	0	0	0	0	0
Highway Fund	737,000	376,000	0	0	0	193,000	168,000	0	0
Tax-Supported Bond	102,000	102,000	0	0	0	0	0	0	0
<b>Total Funding</b>	<b>847,000</b>	<b>486,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>193,000</b>	<b>168,000</b>	<b>0</b>	<b>0</b>

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# Solid Waste

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*Solid Waste*  
*Capital Improvement Ten Year Summary*  
*Fiscal Year 2021 - 2030*

		Budget Year				Ten Year Capital Program				
Page	Project	Total	Prior Appr.	FY 2021	FY 2022	FY 2023	FY 2024	FY 2025	FY 2026	Future
Project Costs										
Solid Waste										
196	Contingency - Solid Waste	458,000	83,000	0	0	30,000	30,000	35,000	35,000	245,000
197	Close Out Cap - Rubblefill	2,091,000	0	190,000	1,901,000	0	0	0	0	0
198	SW Equipment & Vehicle Replacement	576,698	285,698	27,000	28,000	28,000	29,000	29,000	30,000	120,000
199	City/County Groundwater Investigation	156,000	0	156,000	0	0	0	0	0	0
200	40 West Landfill - Cell 5 Construction	4,083,000	0	0	0	0	440,000	3,472,000	171,000	0
201	40 West Security Upgrades	60,000	0	0	60,000	0	0	0	0	0
Solid Waste Total		7,424,698	368,698	373,000	1,989,000	58,000	499,000	3,536,000	236,000	365,000
Funding Sources										
	Solid Waste Fund	1,250,698	368,698	183,000	88,000	58,000	59,000	64,000	65,000	365,000
	Self-Supported Bond	5,309,000	0	190,000	1,036,000	0	440,000	3,472,000	171,000	0
	State Grant	865,000	0	0	865,000	0	0	0	0	0
		7,424,698	368,698	373,000	1,989,000	58,000	499,000	3,536,000	236,000	365,000



<b>Project Title</b>	<b>Contingency - Solid Waste</b>
<b>Project ID</b>	ADM009
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$0

**Project Description** The purpose of this project is to maintain a budgetary reserve to provide for emergency or unanticipated expenditures.

**Project Justification** Due to the inherent uncertainty in estimating capital project costs, it is the policy of the County to maintain project contingency. In the event that the capital contingency accounts have excess funds, the annual appropriation for that purpose may be modified to reflect the source of funds as determined through the budget process.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Contingency	458,000	83,000	0	0	30,000	30,000	35,000	35,000	245,000
<b>Total Cost</b>	<b>458,000</b>	<b>83,000</b>	<b>0</b>	<b>0</b>	<b>30,000</b>	<b>30,000</b>	<b>35,000</b>	<b>35,000</b>	<b>245,000</b>
<b><u>Funding Sources:</u></b>									
Solid Waste Fund	458,000	83,000	0	0	30,000	30,000	35,000	35,000	245,000
<b>Total Funding</b>	<b>458,000</b>	<b>83,000</b>	<b>0</b>	<b>0</b>	<b>30,000</b>	<b>30,000</b>	<b>35,000</b>	<b>35,000</b>	<b>245,000</b>

<b>Project Title</b>	<b>Close Out Cap - Rubble Fill</b>
<b>Project ID</b>	CAP003
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$0



<b>Project Description</b>	The purpose of the project is to design and construct a close out cap for the Rubble landfill.
<b>Project Assumptions</b>	It was decided in 2019 to not renew the Solid Waste Permit for the Rubble Landfill. In accordance with the Code of Maryland Regulations (COMAR), once a landfill is no longer covered under an active permit, the design for the Closure Cap must begin within 24 months and construction must be complete within 36 months.
<b>Project Justification</b>	The work is required to comply with State regulations and the facility permit. Federal regulations do not apply to rubble fill sites.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	1,730,000	0	0	1,730,000	0	0	0	0	0
Engineering and Design	190,000	0	190,000	0	0	0	0	0	0
Inspection	171,000	0	0	171,000	0	0	0	0	0
<b>Total Cost</b>	<b>2,091,000</b>	<b>0</b>	<b>190,000</b>	<b>1,901,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
Self-Supported Bond	1,226,000	0	190,000	1,036,000	0	0	0	0	0
State Grant	865,000	0	0	865,000	0	0	0	0	0
<b>Total Funding</b>	<b>2,091,000</b>	<b>0</b>	<b>190,000</b>	<b>1,901,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

<b>Project Title</b>	<b>SW Equipment &amp; Vehicle Replacement Program</b>
<b>Project ID</b>	VEH011
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$0

**Project Description** The project will replace heavy equipment and vehicles that are beyond their useful life, which are typically the costliest to maintain. In line with the objectives of a well-managed organization, Solid Waste wants to establish an equipment and fleet replacement program that will, in time, set the vehicle and equipment replacement cycle in line with best practices, minimizing operating and maintenance costs.

**Project Justification** The goal of the replacement program is to strike a balance for minimizing replacement costs versus maintenance and fuel costs. The program allows for the purchase of replacement vehicles and equipment used to provide services.

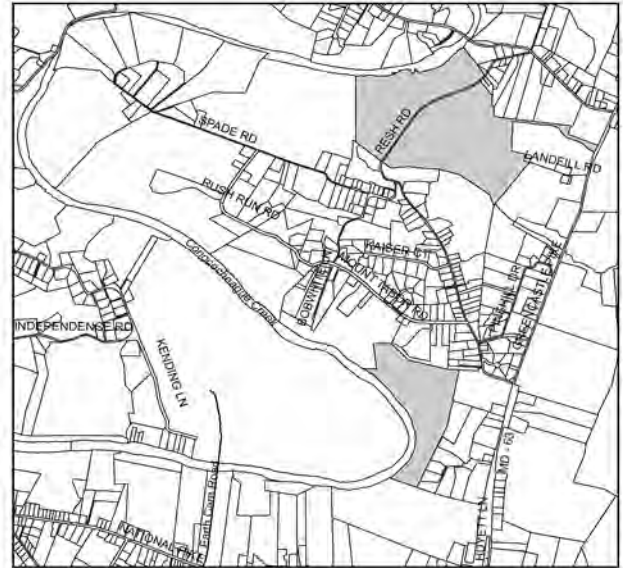
	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Vehicles	576,698	285,698	27,000	28,000	28,000	29,000	29,000	30,000	120,000
<b>Total Cost</b>	<b>576,698</b>	<b>285,698</b>	<b>27,000</b>	<b>28,000</b>	<b>28,000</b>	<b>29,000</b>	<b>29,000</b>	<b>30,000</b>	<b>120,000</b>
<b><u>Funding Sources:</u></b>									
Solid Waste Fund	576,698	285,698	27,000	28,000	28,000	29,000	29,000	30,000	120,000
<b>Total Funding</b>	<b>576,698</b>	<b>285,698</b>	<b>27,000</b>	<b>28,000</b>	<b>28,000</b>	<b>29,000</b>	<b>29,000</b>	<b>30,000</b>	<b>120,000</b>

**Project Title** City/County Landfill Ground Water Investigation

**Project ID** STY024

**Full-time Employees** 0

**Operating Costs** \$0



**Project Description** Investigation to determine if the groundwater influences the leachate production at the unlined landfill.

**Project Justification** Groundwater mapping indicates the flow of groundwater through the site may be influencing the production of leachate at the landfill.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
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**Project Costs:**

Engineering and Design	156,000	0	156,000	0	0	0	0	0	0
<b>Total Cost</b>	<b>156,000</b>	<b>0</b>	<b>156,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

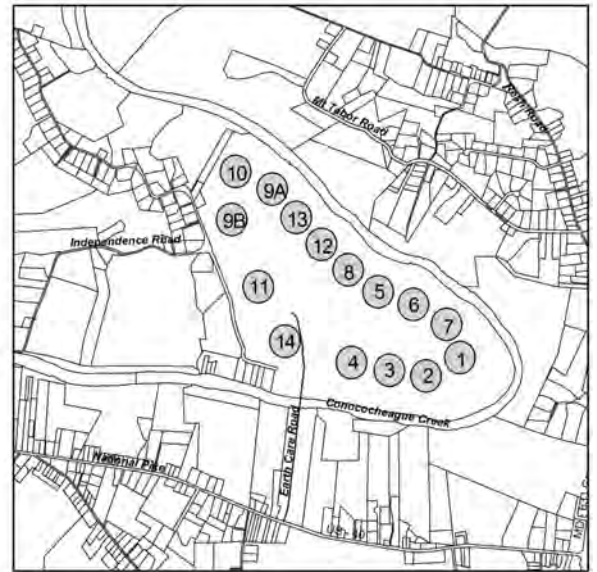
**Funding Sources:**

Solid Waste Fund	156,000	0	156,000	0	0	0	0	0	0
<b>Total Funding</b>	<b>156,000</b>	<b>0</b>	<b>156,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

**Project Title**                      **40 West Landfill - Cell 5 Construction**

**Full-time Employees**        0

**Operating Costs**                \$200,000



**Project Description**            Design, construction and inspection of a landfill cell at the 40 West Landfill.

**Project Assumptions**        This project will be needed if no recycling facility has been built at 40 West or no other method of dealing with solid waste has been adopted.

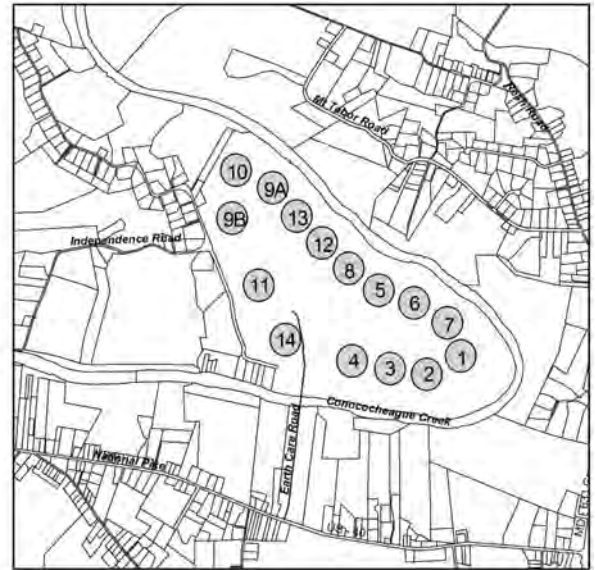
**Project Justification**        Cell 5 is the next cell in the progression of the future build out of the 40 West Landfill. This cell will be needed to continue regular operations.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Construction	3,474,000	0	0	0	0	0	3,360,000	114,000	0
Engineering and Design	440,000	0	0	0	0	440,000	0	0	0
Inspection	169,000	0	0	0	0	0	112,000	57,000	0
<b>Total Cost</b>	<b>4,083,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>440,000</b>	<b>3,472,000</b>	<b>171,000</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
Self-Supported Bond	4,083,000	0	0	0	0	440,000	3,472,000	171,000	0
<b>Total Funding</b>	<b>4,083,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>440,000</b>	<b>3,472,000</b>	<b>171,000</b>	<b>0</b>

**Project Title**                      **40 West Security Upgrades**

**Full-time Employees**            0

**Operating Costs**                 \$0



**Project Description**            Installation of security cameras at the Solid Waste Administration Building, scale house and maintenance building with coverage of the residential lot.

**Project Assumptions**           This project will be done in accordance with security projects for other County facilities.

**Project Justification**           Employee safety is a concern. This project will help protect employees from unruly customers and protect the site from vandalism or theft.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Construction	60,000	0	0	60,000	0	0	0	0	0
<b>Total Cost</b>	<b>60,000</b>	<b>0</b>	<b>0</b>	<b>60,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
Solid Waste Fund	60,000	0	0	60,000	0	0	0	0	0
<b>Total Funding</b>	<b>60,000</b>	<b>0</b>	<b>0</b>	<b>60,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

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# Transit

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*Transit*  
*Capital Improvement Ten Year Summary*  
*Fiscal Year 2021 - 2030*

Page	Project	Total	Prior Appr.	Budget Year		Ten Year Capital Program				
				FY 2021	FY 2022	FY 2023	FY 2024	FY 2025	FY 2026	Future
Project Costs										
<u>Transit</u>										
204	Vehicle Preventive Maintenance	4,234,230	484,230	375,000	375,000	375,000	375,000	375,000	375,000	1,500,000
205	Fixed Route Bus Replacement Program	6,580,000	3,972,000	0	652,000	0	0	0	0	1,956,000
206	ADA Bus Replacement	540,000	0	90,000	0	90,000	90,000	0	90,000	180,000
207	Support Vehicles	80,000	0	80,000	0	0	0	0	0	0
208	Transportation Development Plan	192,000	92,000	0	0	0	0	0	0	100,000
Transit Total		11,626,230	4,548,230	545,000	1,027,000	465,000	465,000	375,000	465,000	3,736,000
Funding Sources										
	General Fund	968,923	320,923	0	102,000	46,000	46,000	37,000	46,000	371,000
	Capital Reserve - General	189,000	135,000	54,000	0	0	0	0	0	0
	Federal Grant	9,299,384	3,636,384	436,000	822,000	372,000	372,000	300,000	372,000	2,989,000
	State Grant	1,168,923	455,923	55,000	103,000	47,000	47,000	38,000	47,000	376,000
		11,626,230	4,548,230	545,000	1,027,000	465,000	465,000	375,000	465,000	3,736,000

Project Title	Vehicle Maintenance Program
Project ID	EQP021
Full-time Employees	0
Operating Costs	\$0

Project Description	This project will provide for preventative maintenance and repairs of transit vehicles and facilities.
Project Assumptions	The project assumes funding 80% Federal, 10% State, 10% Local and is contingent on grant funding. Federal Transit Administration (FTA) determinations now allow for expenses such as tools, equipment, repair materials, and preventative care for the fleet of transit vehicles and transit facilities to be capitalized.
Project Justification	Maintaining federally funded assets is a priority to the Federal Transit Administration (FTA), Maryland Transit Administration (MTA) and Washington County Transit (WCT).

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Equipment/Furniture	4,234,230	484,230	375,000	375,000	375,000	375,000	375,000	375,000	1,500,000
<b>Total Cost</b>	<b>4,234,230</b>	<b>484,230</b>	<b>375,000</b>	<b>375,000</b>	<b>375,000</b>	<b>375,000</b>	<b>375,000</b>	<b>375,000</b>	<b>1,500,000</b>
<b>Funding Sources:</b>									
General Fund	343,923	10,923	0	37,000	37,000	37,000	37,000	37,000	148,000
Capital Reserve - General	74,000	37,000	37,000	0	0	0	0	0	0
Federal Grant	3,387,384	387,384	300,000	300,000	300,000	300,000	300,000	300,000	1,200,000
State Grant	428,923	48,923	38,000	38,000	38,000	38,000	38,000	38,000	152,000
<b>Total Funding</b>	<b>4,234,230</b>	<b>484,230</b>	<b>375,000</b>	<b>375,000</b>	<b>375,000</b>	<b>375,000</b>	<b>375,000</b>	<b>375,000</b>	<b>1,500,000</b>

<b>Project Title</b>	<b>Fixed Route Bus Replacement Program</b>
<b>Project ID</b>	VEH003
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$0

<b>Project Description</b>	FY 22 - Replace two (2) fixed-route transit buses, Units 713 - 714 which will become 725 - 726 FY 30 - Replace six (6) fixed-route transit buses, Units 715- 720 which will become 727 - 732. FY 31 - Replace one (1) fixed-route transit buses, Unit 721 which will become 733.
<b>Project Assumptions</b>	All out-year procurements will be based on available grant funds from MTA and local match approval. Dollar figures shown are provided by MTA based on the current small/mid-size bus contract. The project funding assumes 80% Federal, 10% State, 10% Local.
<b>Project Justification</b>	Following the Maryland Transit Administration (MTA) useful life criteria, MTA provides grant funding to replace existing vehicles when they reach the end of the useful life criteria. Out years are projected based on the current useful life schedule.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Vehicles	6,580,000	3,972,000	0	652,000	0	0	0	0	1,956,000
<b>Total Cost</b>	<b>6,580,000</b>	<b>3,972,000</b>	<b>0</b>	<b>652,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1,956,000</b>
<b><u>Funding Sources:</u></b>									
General Fund	560,000	300,000	0	65,000	0	0	0	0	195,000
Capital Reserve - General	98,000	98,000	0	0	0	0	0	0	0
Federal Grant	5,264,000	3,177,000	0	522,000	0	0	0	0	1,565,000
State Grant	658,000	397,000	0	65,000	0	0	0	0	196,000
<b>Total Funding</b>	<b>6,580,000</b>	<b>3,972,000</b>	<b>0</b>	<b>652,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1,956,000</b>

<b>Project Title</b>	<b>ADA Bus Replacement</b>
<b>Project ID</b>	VEH005
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$0

<b>Project Description</b>	FY 21 - Replace one (1) ADA Program bus (Unit 505) becomes 508 FY 23 - Replace one (1) ADA Program bus (Unit 506) becomes 509 FY 24 - Replace one (1) ADA Program bus (Unit 507) becomes 510 FY 26 - Replace one (1) ADA Program bus (Unit 508) becomes 511 FY 28 - Replace one (1) ADA Program bus (Unit 509) becomes 512 FY 29 - Replace one (1) ADA Program bus (Unit 510) becomes 513 FY 31 - Replace one (1) ADA Program bus (Unit 511) becomes 514
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<b>Project Assumptions</b>	All out-year procurements will be based on available grant funds from MTA and local match approval. Dollar figures shown are provided by MTA based on the current small/mid-size bus contract. The project funding assumes 80% Federal, 10% State, 10% Local.
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<b>Project Justification</b>	Following the Maryland Transit Administration (MTA) useful life criteria, MTA provides grant funding to replace existing vehicles when they reach the end of the useful life criteria. Out years are projected based on the current useful life schedule.
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	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Vehicles	540,000	0	90,000	0	90,000	90,000	0	90,000	180,000
<b>Total Cost</b>	<b>540,000</b>	<b>0</b>	<b>90,000</b>	<b>0</b>	<b>90,000</b>	<b>90,000</b>	<b>0</b>	<b>90,000</b>	<b>180,000</b>
<b><u>Funding Sources:</u></b>									
General Fund	45,000	0	0	0	9,000	9,000	0	9,000	18,000
Capital Reserve - General	9,000	0	9,000	0	0	0	0	0	0
Federal Grant	432,000	0	72,000	0	72,000	72,000	0	72,000	144,000
State Grant	54,000	0	9,000	0	9,000	9,000	0	9,000	18,000
<b>Total Funding</b>	<b>540,000</b>	<b>0</b>	<b>90,000</b>	<b>0</b>	<b>90,000</b>	<b>90,000</b>	<b>0</b>	<b>90,000</b>	<b>180,000</b>

<b>Project Title</b>	<b>Support Vehicles</b>
<b>Project ID</b>	VEH014
<b>Full-time Employees</b>	0
<b>Operating Costs</b>	\$0

<b>Project Description</b>	FY21 – Replace 2 Transit Vans These vehicles are used to shuttle drivers to/from the Transfer Center at the beginning/end of their daily work assignments along with providing back-up vehicles for all Transit programs.
<b>Project Assumptions</b>	All procurements are based on available grant funds from MTA and local match approval. Dollar figures shown are based on the proposed MTA van contract which is due to be awarded in March 2020. The project funding issues 80 Federal, 10 % State and 10% Local.
<b>Project Justification</b>	Following the Maryland Transit Administration (MTA) useful life criteria, MTA provides grant funding to replace existing vehicles when they reach the end of their useful lift criteria.

	<b>Total</b>	<b>Prior Appr.</b>	<b>2021</b>	<b>2022</b>	<b>2023</b>	<b>2024</b>	<b>2025</b>	<b>2026</b>	<b>Future</b>
<b><u>Project Costs:</u></b>									
Vehicles	80,000	0	80,000	0	0	0	0	0	0
<b>Total Cost</b>	<b>80,000</b>	<b>0</b>	<b>80,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
Capital Reserve - General	8,000	0	8,000	0	0	0	0	0	0
Federal Grant	64,000	0	64,000	0	0	0	0	0	0
State Grant	8,000	0	8,000	0	0	0	0	0	0
<b>Total Funding</b>	<b>80,000</b>	<b>0</b>	<b>80,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

Project Title	Transportation Development Plan
Project ID	STY016
Full-time Employees	0
Operating Costs	\$0

Project Description	The Transportation Development Plan (TDP) is a 5-year plan that provides a short-range vision of public transportation in Washington County and help guide transportation decisions over the next five years.
Project Assumptions	The project assumes funding 80% Federal, 10% State, 10% Local and is contingent on grant funding.
Project Justification	The Maryland Transit Administration (MTA) requires the Locally Operated Transit Systems (LOTS) in Maryland to conduct a Transportation Development Plan every five years. The LOTS then use their TDPs as abasis for preparing their Annual Transportation Plan (ATP) to obtain grant funding.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Other	192,000	92,000	0	0	0	0	0	0	100,000
Total Cost	192,000	92,000	0	0	0	0	0	0	100,000
<b>Funding Sources:</b>									
General Fund	20,000	10,000	0	0	0	0	0	0	10,000
Federal Grant	152,000	72,000	0	0	0	0	0	0	80,000
State Grant	20,000	10,000	0	0	0	0	0	0	10,000
Total Funding	192,000	92,000	0	0	0	0	0	0	100,000

# Water Quality

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*Water Quality  
Capital Improvement Ten Year Summary  
Fiscal Year 2021 - 2030*

		Budget Year				Ten Year Capital Program				
Page	Project	Total	Prior Appr.	FY 2021	FY 2022	FY 2023	FY 2024	FY 2025	FY 2026	Future
Project Costs										
Water Quality										
210	Contingency - Utility Admin	166,677	135,677	0	0	0	0	0	0	31,000
211	General Building Improvements	964,000	0	0	0	0	370,000	594,000	0	0
212	Lab Equipment Replacement	354,117	30,117	105,000	25,000	25,000	23,000	23,000	24,000	99,000
213	WQ Equip/Vehicle Replacement Program	1,107,320	252,320	75,000	80,000	80,000	80,000	85,000	85,000	370,000
214	Contingency - Sewer	86,939	36,939	0	0	0	0	0	0	50,000
215	Replace Grinder Pumps	890,418	50,418	60,000	80,000	80,000	80,000	80,000	90,000	370,000
216	Pen Mar/ Highfield/ Cascade Septic Tank Pumping and Replacement	115,000	75,000	40,000	0	0	0	0	0	0
217	Pump Station Upgrades - Various Stations	4,340,803	1,527,803	275,000	0	153,000	0	0	885,000	1,500,000
218	Collection System Rehabilitation Project	3,452,618	372,618	0	560,000	0	0	870,000	0	1,650,000
219	Capacity Management Project	10,900,542	4,460,542	6,440,000	0	0	0	0	0	0
220	Smithsburg WWTP ENR Upgrade	12,903,387	4,403,387	3,000,000	0	0	0	0	0	5,500,000
221	Heavy Sewer EQP and VEH Replacement	893,934	361,934	94,000	94,000	94,000	35,000	35,000	35,000	145,000
222	Potomac Edison Pump Station & Force Main	1,700,000	0	0	0	0	0	0	0	1,700,000
223	General WwTP Improvements	1,350,000	0	0	0	250,000	250,000	300,000	250,000	300,000
224	Sharpsburg Water Meter Cradle Replacement	1,000,000	250,000	125,000	250,000	250,000	125,000	0	0	0
225	Water Meter Replacement	260,339	110,339	15,000	15,000	15,000	15,000	15,000	15,000	60,000
226	Mt Aetna Water System Improvements	729,000	130,000	0	0	599,000	0	0	0	0
227	Cascade Town Center Water System Improvements	125,000	0	125,000	0	0	0	0	0	0
228	Sharpsburg Water Treatment Plant	794,000	204,000	0	0	0	0	0	590,000	0
229	General WTP Improvements	1,168,298	66,298	30,000	0	242,000	290,000	0	0	540,000
230	Highfield/Sharpsburg Water Storage Tank	336,000	0	0	0	0	0	0	0	336,000
231	WQ Main Replacement	1,366,000	0	0	0	566,000	100,000	100,000	0	600,000
Water Quality Total		45,004,392	12,467,392	10,384,000	1,104,000	2,354,000	1,368,000	2,102,000	1,974,000	13,251,000
Funding Sources										
	General Fund	504,400	29,400	0	175,000	175,000	125,000	0	0	0
	Cascade Fund	13,000	13,000	0	0	0	0	0	0	0
	Utility Admin Fund	1,598,714	388,714	180,000	105,000	105,000	103,000	108,000	109,000	500,000
	Water Fund	276,339	126,339	15,000	15,000	15,000	15,000	15,000	15,000	60,000
	Sewer Fund	3,755,828	2,293,828	194,000	174,000	174,000	115,000	115,000	125,000	565,000
	Self-Supported Bond	35,406,111	9,341,111	9,820,000	460,000	1,710,000	985,000	1,539,000	1,725,000	9,826,000
	Capital Reserve - General	50,000	0	50,000	0	0	0	0	0	0
	Capital Reserve - Water	125,000	125,000	0	0	0	0	0	0	0
	State Grant	1,575,000	150,000	125,000	175,000	175,000	25,000	325,000	0	600,000
	Contributions	1,700,000	0	0	0	0	0	0	0	1,700,000
		45,004,392	12,467,392	10,384,000	1,104,000	2,354,000	1,368,000	2,102,000	1,974,000	13,251,000



Project Title	Contingency - Utility Admin
Project ID	ADM003
Full-time Employees	0
Operating Costs	\$0

**Project Description**      The project provides a budgetary reserve for emergency or unanticipated expenditures.

**Project Justification**      Due to the inherent uncertainty in estimating capital project costs, it is the policy of the County to maintain project contingency. In the event that the capital contingency accounts have excess funds, the annual appropriation for the purpose may be modified to reflect the source of funds as determined through the budget process.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Contingency	166,677	135,677	0	0	0	0	0	0	31,000
<b>Total Cost</b>	<b>166,677</b>	<b>135,677</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>31,000</b>
<b><u>Funding Sources:</u></b>									
General Fund	29,400	29,400	0	0	0	0	0	0	0
Utility Admin Fund	137,277	106,277	0	0	0	0	0	0	31,000
<b>Total Funding</b>	<b>166,677</b>	<b>135,677</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>31,000</b>

Project Title

General Building Improvements

Project ID

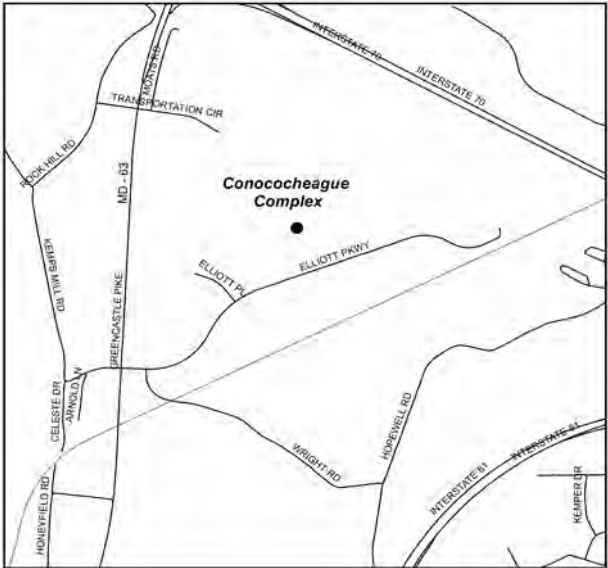
BLD073

Full-time Employees

0

Operating Costs

\$0



Project Description

The project will provide for general building improvements to the administration office building and maintenance buildings as well as associated paving on the Conococheague Complex.

Project Justification

The buildings are in need of improvements due to normal aging and recommendations by the manufacturer of the building.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	964,000	0	0	0	0	370,000	594,000	0	0
<b>Total Cost</b>	<b>964,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>370,000</b>	<b>594,000</b>	<b>0</b>	<b>0</b>
<b>Funding Sources:</b>									
Self-Supported Bond	964,000	0	0	0	0	370,000	594,000	0	0
<b>Total Funding</b>	<b>964,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>370,000</b>	<b>594,000</b>	<b>0</b>	<b>0</b>

Project Title	Lab Equipment Replacement
Project ID	EQP063
Full-time Employees	0
Operating Costs	\$0

**Project Description**      The project will replace / upgrade existing lab testing equipment.

**Project Justification**      The existing lab equipment has reached its useful life and needs to be replaced / upgraded.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Equipment/Furniture	354,117	30,117	105,000	25,000	25,000	23,000	23,000	24,000	99,000
<b>Total Cost</b>	<b>354,117</b>	<b>30,117</b>	<b>105,000</b>	<b>25,000</b>	<b>25,000</b>	<b>23,000</b>	<b>23,000</b>	<b>24,000</b>	<b>99,000</b>
<b><u>Funding Sources:</u></b>									
Utility Admin Fund	354,117	30,117	105,000	25,000	25,000	23,000	23,000	24,000	99,000
<b>Total Funding</b>	<b>354,117</b>	<b>30,117</b>	<b>105,000</b>	<b>25,000</b>	<b>25,000</b>	<b>23,000</b>	<b>23,000</b>	<b>24,000</b>	<b>99,000</b>

Project Title	WQ Equip/Vehicle Replacement Program
Project ID	VEH007
Full-time Employees	0
Operating Costs	\$0

Project Description	The project will replace heavy equipment and vehicles that are beyond their useful life, which are typically the costliest to maintain. In line with the objectives of a well-managed organization, Water Quality wants to establish an equipment and fleet replacement program that will, in time, set the vehicle and equipment replacement cycle in line with best practices, minimizing operating and maintenance costs.
Project Justification	The goal of the replacement program is to strike a balance for minimizing replacement costs versus maintenance and fuel costs. The program allows for the purchase of replacement vehicles and equipment used to provide services within the Water Quality service areas.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Equipment/Furniture	50,000	50,000	0	0	0	0	0	0	0
Vehicles	1,057,320	202,320	75,000	80,000	80,000	80,000	85,000	85,000	370,000
<b>Total Cost</b>	<b>1,107,320</b>	<b>252,320</b>	<b>75,000</b>	<b>80,000</b>	<b>80,000</b>	<b>80,000</b>	<b>85,000</b>	<b>85,000</b>	<b>370,000</b>
<b>Funding Sources:</b>									
Utility Admin Fund	1,107,320	252,320	75,000	80,000	80,000	80,000	85,000	85,000	370,000
<b>Total Funding</b>	<b>1,107,320</b>	<b>252,320</b>	<b>75,000</b>	<b>80,000</b>	<b>80,000</b>	<b>80,000</b>	<b>85,000</b>	<b>85,000</b>	<b>370,000</b>

Project Title	Contingency - Sewer
Project ID	ADM008
Full-time Employees	0
Operating Costs	\$0

**Project Description**      This project is a budgetary reserve to provide for emergency or unanticipated expenditures.

**Project Justification**      Due to the inherent uncertainty in estimating capital project costs, it is the policy of the County to maintain project contingency. In the event that the capital contingency accounts have excess funds, the annual appropriation for the purpose may be modified to reflect the source of funds as determined through the budget process.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Contingency	86,939	36,939	0	0	0	0	0	0	50,000
<b>Total Cost</b>	<b>86,939</b>	<b>36,939</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>50,000</b>
<b>Funding Sources:</b>									
Sewer Fund	86,939	36,939	0	0	0	0	0	0	50,000
<b>Total Funding</b>	<b>86,939</b>	<b>36,939</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>50,000</b>

Project Title	Replace Grinder Pumps
Project ID	EQP055
Full-time Employees	0
Operating Costs	\$0

**Project Description**      The project will provide the replacement of core units of aging infrastructure and will extend the life of the system.

**Project Justification**      Some of the existing grinder pumps have reached their life expectancy and need to be replaced.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Equipment/Furniture	890,418	50,418	60,000	80,000	80,000	80,000	80,000	90,000	370,000
<b>Total Cost</b>	<b>890,418</b>	<b>50,418</b>	<b>60,000</b>	<b>80,000</b>	<b>80,000</b>	<b>80,000</b>	<b>80,000</b>	<b>90,000</b>	<b>370,000</b>
<b><u>Funding Sources:</u></b>									
Sewer Fund	890,418	50,418	60,000	80,000	80,000	80,000	80,000	90,000	370,000
<b>Total Funding</b>	<b>890,418</b>	<b>50,418</b>	<b>60,000</b>	<b>80,000</b>	<b>80,000</b>	<b>80,000</b>	<b>80,000</b>	<b>90,000</b>	<b>370,000</b>

Project Title

Pen Mar / Highfield / Cascade Septic Tank Pumping and Replacement

Project ID

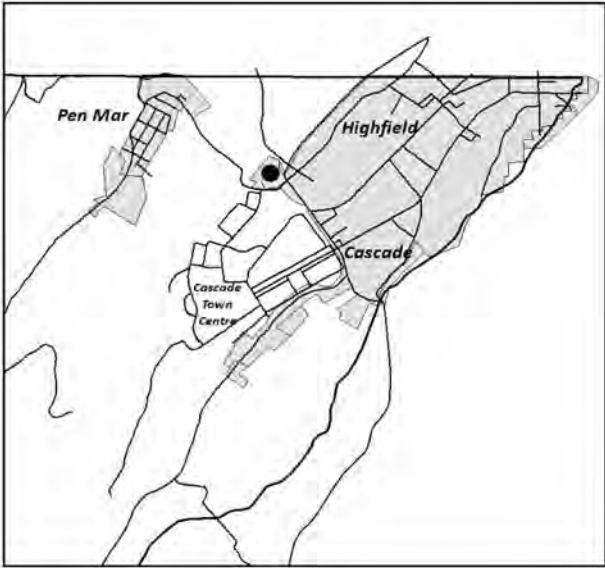
EQP065

Full-time Employees

0

Operating Costs

\$0



Project Description

Pump the solids out of the existing septic tanks and replace damaged tanks.

Project Assumptions

Advertise this project to have a private septic hauler perform the work.

Project Justification

The Pen Mar / Highfield / Cascade sewer collection system was constructed as a hybrid collection system. The property owners had existing on-lot septic systems; thus, they were permitted to keep their septic tanks. The sewage from the homes flows into the existing septic tank where the solids are collected. The effluent from the septic tank then flows into a new pump tank that will discharge into a small diameter gravity system. The solids will accumulate in the septic tank until they are required to be pumped out. When the County installed the sewer system, we assumed the responsibility of pumping out the septic tanks. It has been 10 years since the tanks were last pumped out.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	115,000	75,000	40,000	0	0	0	0	0	0
Total Cost	115,000	75,000	40,000	0	0	0	0	0	0
<b>Funding Sources:</b>									
Sewer Fund	115,000	75,000	40,000	0	0	0	0	0	0
Total Funding	115,000	75,000	40,000	0	0	0	0	0	0

Project Title	Pump Station Upgrades - Various Stations
Project ID	LIN034
Full-time Employees	0
Operating Costs	\$0

**Project Description**      The project includes electrical and equipment upgrades.

**Project Justification**      Upgrades are required to address aging equipment issues.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Construction	4,340,803	1,527,803	275,000	0	153,000	0	0	885,000	1,500,000
<b>Total Cost</b>	<b>4,340,803</b>	<b>1,527,803</b>	<b>275,000</b>	<b>0</b>	<b>153,000</b>	<b>0</b>	<b>0</b>	<b>885,000</b>	<b>1,500,000</b>
<b><u>Funding Sources:</u></b>									
Sewer Fund	4,683	4,683	0	0	0	0	0	0	0
Self-Supported Bond	4,336,120	1,523,120	275,000	0	153,000	0	0	885,000	1,500,000
<b>Total Funding</b>	<b>4,340,803</b>	<b>1,527,803</b>	<b>275,000</b>	<b>0</b>	<b>153,000</b>	<b>0</b>	<b>0</b>	<b>885,000</b>	<b>1,500,000</b>

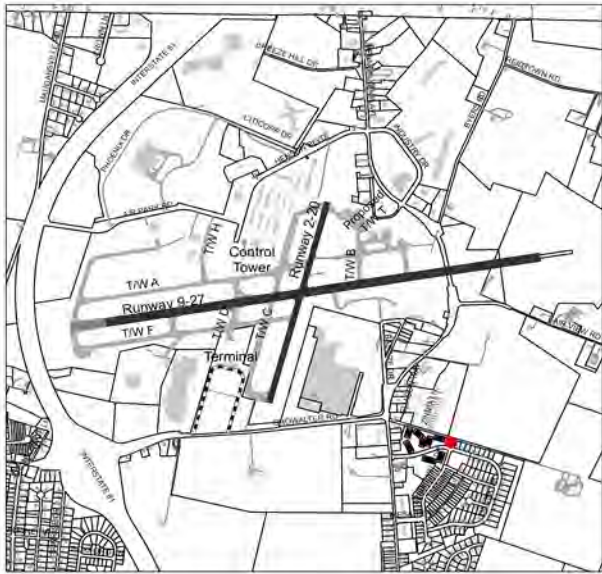


Project Title	Collection System Rehabilitation Project
Project ID	LIN040
Full-time Employees	0
Operating Costs	\$0

Project Description	This project will rehabilitate existing sewer lines.
Project Assumptions	Grant estimate based on last I&I project. Grant funds estimated at 35%.
Project Justification	The Collection Systems in the County are aging and showing normal deterioration which requires repairs, slip lining or replacement to reduce the amount of Inflow and Infiltration (I&I) entering the system and to reduce the potential of line breaks and sewer overflows. I&I impacts the budget by increasing the amount of water requiring treatment without increasing the revenue to pay for this expense. Sewer line breaks and sewer overflows cause environmental issues.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	3,452,618	372,618	0	560,000	0	0	870,000	0	1,650,000
<b>Total Cost</b>	<b>3,452,618</b>	<b>372,618</b>	<b>0</b>	<b>560,000</b>	<b>0</b>	<b>0</b>	<b>870,000</b>	<b>0</b>	<b>1,650,000</b>
<b>Funding Sources:</b>									
Sewer Fund	340,235	340,235	0	0	0	0	0	0	0
Self-Supported Bond	2,137,383	32,383	0	460,000	0	0	570,000	0	1,075,000
State Grant	975,000	0	0	100,000	0	0	300,000	0	575,000
<b>Total Funding</b>	<b>3,452,618</b>	<b>372,618</b>	<b>0</b>	<b>560,000</b>	<b>0</b>	<b>0</b>	<b>870,000</b>	<b>0</b>	<b>1,650,000</b>

Project Title	Capacity Management Project
Project ID	LIN042
Full-time Employees	0
Operating Costs	\$0



**Project Description** Capacity Management has been expanded to include the following: Phase I – Construct a new regional pump station to service the Maugansville area. The new pump station will pump the sewage to the Cedar Lawn area where it will be discharged into the County's gravity collection system. Gravity lines will be constructed from the new station to Maugansville and Maugans Meadow's pump stations. Once the new station is on-line the existing Maugansville and Maugans Meadow's pump stations will be taken off-line. Phase II – Construct a new gravity line from the regional station to the Garden Spot pump station. At this time, the Garden Spot pump station will be taken off-line. Phase III – Construct a new gravity line from Garden Spot pump station to Freedom Hills pump station. At this time, the Freedom Hill pump station will be taken off-line.

**Project Justification** Currently the Maugansville Collection area is approaching its design capacity. This project will redirect flow from this area to another drainage area providing capacity relief and facilitating economic growth in the drainage area.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	10,380,542	3,940,542	6,440,000	0	0	0	0	0	0
Land Acquisition	520,000	520,000	0	0	0	0	0	0	0
<b>Total Cost</b>	<b>10,900,542</b>	<b>4,460,542</b>	<b>6,440,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>Funding Sources:</b>									
Sewer Fund	74,567	74,567	0	0	0	0	0	0	0
Self-Supported Bond	10,825,975	4,385,975	6,440,000	0	0	0	0	0	0
<b>Total Funding</b>	<b>10,900,542</b>	<b>4,460,542</b>	<b>6,440,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

Project Title

Smithsburg WwTP - ENR Upgrades

Project ID

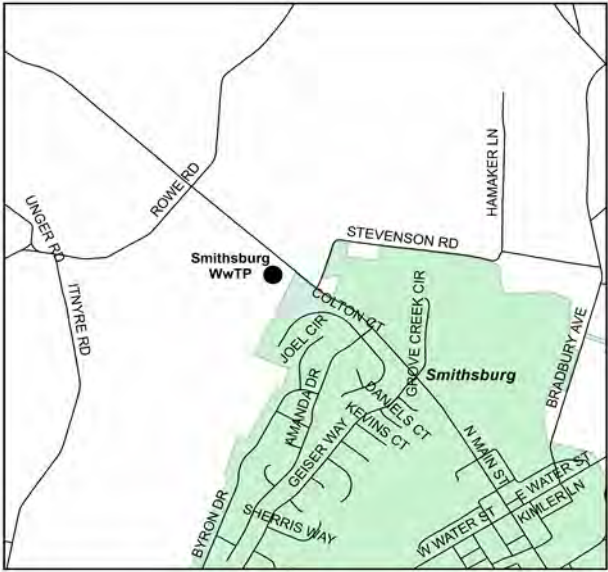
TRP021

Full-time Employees

0

Operating Costs

\$10,000



Project Description

The project will upgrade the facility to address Maryland Department of the Environment (MDE) strategy for Enhanced Nutrient Removal (ENR) and expand capacity to address growth needs of the area.

Project Assumptions

Pending grant funding approval.

Project Justification

The upgrade is required to meet MDE's ENR strategy and to prepare for anticipated development in the area.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	11,259,917	3,259,917	3,000,000	0	0	0	0	0	5,000,000
Engineering and Design	1,643,470	1,143,470	0	0	0	0	0	0	500,000
<b>Total Cost</b>	<b>12,903,387</b>	<b>4,403,387</b>	<b>3,000,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>5,500,000</b>
<b>Funding Sources:</b>									
Sewer Fund	1,350,052	1,350,052	0	0	0	0	0	0	0
Self-Supported Bond	11,553,335	3,053,335	3,000,000	0	0	0	0	0	5,500,000
<b>Total Funding</b>	<b>12,903,387</b>	<b>4,403,387</b>	<b>3,000,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>5,500,000</b>

Project Title	Heavy Sewer EQP and VEH Replacement
Project ID	VEH010
Full-time Employees	0
Operating Costs	\$0

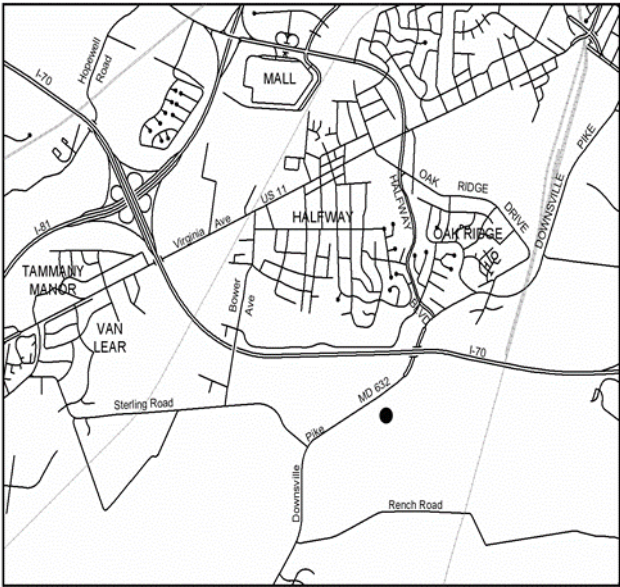
Project Description	The project will replace heavy equipment and vehicles that are beyond their useful life, which are typically the costliest to maintain. In line with the objectives of a well-managed organization, Water Quality strives to establish an equipment and fleet replacement program that will, in time, set the vehicle and equipment replacement cycle in line with best practices, minimizing operating and maintenance costs.
Project Justification	The goal of the replacement program is to strike a balance for minimizing replacement costs versus maintenance and fuel costs. The program allows for the purchase of replacement vehicles and equipment used to provide services within the Water Quality service areas.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Equipment/Furniture	48,000	4,000	4,000	4,000	4,000	5,000	5,000	5,000	17,000
Other	22,962	22,962	0	0	0	0	0	0	0
Vehicles	822,972	334,972	90,000	90,000	90,000	30,000	30,000	30,000	128,000
Total Cost	893,934	361,934	94,000	94,000	94,000	35,000	35,000	35,000	145,000
<b>Funding Sources:</b>									
Sewer Fund	893,934	361,934	94,000	94,000	94,000	35,000	35,000	35,000	145,000
Total Funding	893,934	361,934	94,000	94,000	94,000	35,000	35,000	35,000	145,000

**Project Title** Potomac Edison Pump Station & Force Main

**Full-time Employees** 0

**Operating Costs** \$0



**Project Description** The project will provide for the relocation of the existing force main and the Potomac Edison pump station.

**Project Assumptions** Pending developer contributions.

**Project Justification** Relocation of the Potomac Edison pump station and force main is required to facilitate service of the properties in the revised Urban Growth Area Boundary.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	1,700,000	0	0	0	0	0	0	0	1,700,000
<b>Total Cost</b>	<b>1,700,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1,700,000</b>
<b>Funding Sources:</b>									
Contributions	1,700,000	0	0	0	0	0	0	0	1,700,000
<b>Total Funding</b>	<b>1,700,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1,700,000</b>

Project Title	General WwTP Improvement
Full-time Employees	0
Operating Costs	\$0

Project Description	The project involves general upgrades including electrical systems and equipment as required to address aging infrastructure and regulatory requirements.
Project Justification	Upgrades are required to address aging infrastructure and equipment. Some of these upgrades may not have been accomplished under previous projects due to grant funding restrictions.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	1,250,000	0	0	0	250,000	250,000	250,000	250,000	250,000
Engineering and Design	100,000	0	0	0	0	0	50,000	0	50,000
<b>Total Cost</b>	<b>1,350,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>250,000</b>	<b>250,000</b>	<b>300,000</b>	<b>250,000</b>	<b>300,000</b>
<b>Funding Sources:</b>									
Self-Supported Bond	1,350,000	0	0	0	250,000	250,000	300,000	250,000	300,000
<b>Total Funding</b>	<b>1,350,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>250,000</b>	<b>250,000</b>	<b>300,000</b>	<b>250,000</b>	<b>300,000</b>

Project Title	Sharpsburg Water Meter Cradle Replacement
Project ID	EQP068
Full-time Employees	0
Operating Costs	\$0

**Project Description** Replace the existing water meter cradles that contain lead.

**Project Justification** The existing fitting associated with the cradles are old and contain lead. EPA has passed a new law that requires all the lead fittings to be removed and replaced with non-lead fitting. Also, the cradles are required to have backflow preventers installed, but the majority of the existing cradles don't have backflow prevents. This project will address both issues by replacing all the existing fitting with non-lead fitting and installation of a backflow preventer.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Construction	1,000,000	250,000	125,000	250,000	250,000	125,000	0	0	0
<b>Total Cost</b>	<b>1,000,000</b>	<b>250,000</b>	<b>125,000</b>	<b>250,000</b>	<b>250,000</b>	<b>125,000</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
General Fund	475,000	0	0	175,000	175,000	125,000	0	0	0
Water Fund	0	0	0	0	0	0	0	0	0
Capital Reserve - Water	125,000	125,000	0	0	0	0	0	0	0
State Grant	400,000	125,000	125,000	75,000	75,000	0	0	0	0
<b>Total Funding</b>	<b>1,000,000</b>	<b>250,000</b>	<b>125,000</b>	<b>250,000</b>	<b>250,000</b>	<b>125,000</b>	<b>0</b>	<b>0</b>	<b>0</b>

Project Title	WQ Water Meter Replacement
Project ID	LIN004
Full-time Employees	0
Operating Costs	\$0

**Project Description**      The project will replace aging water meters in various water distribution systems.

**Project Justification**      Replacement is needed due to the age and deterioration of the water meters.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Construction	260,339	110,339	15,000	15,000	15,000	15,000	15,000	15,000	60,000
<b>Total Cost</b>	<b>260,339</b>	<b>110,339</b>	<b>15,000</b>	<b>15,000</b>	<b>15,000</b>	<b>15,000</b>	<b>15,000</b>	<b>15,000</b>	<b>60,000</b>
<b><u>Funding Sources:</u></b>									
Water Fund	260,339	110,339	15,000	15,000	15,000	15,000	15,000	15,000	60,000
<b>Total Funding</b>	<b>260,339</b>	<b>110,339</b>	<b>15,000</b>	<b>15,000</b>	<b>15,000</b>	<b>15,000</b>	<b>15,000</b>	<b>15,000</b>	<b>60,000</b>



Project Title

Mt Aetna Water System Improvements

Project ID

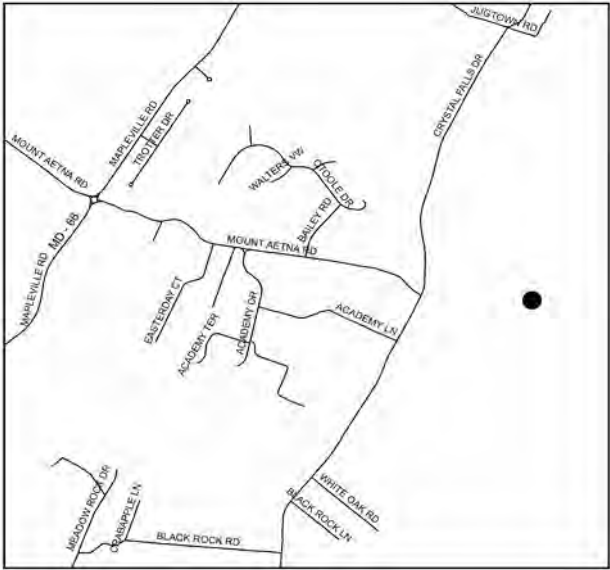
LIN043

Full-time Employees

0

Operating Costs

\$0



Project Description

The project includes general plant and system improvements.

Project Assumptions

Will seek grant funding from MDE.

Project Justification

The project includes the replacement and upgrade of aging infrastructure and manufacturer recommendations.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	729,000	130,000	0	0	599,000	0	0	0	0
<b>Total Cost</b>	<b>729,000</b>	<b>130,000</b>	<b>0</b>	<b>0</b>	<b>599,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>Funding Sources:</b>									
Self-Supported Bond	654,000	105,000	0	0	549,000	0	0	0	0
State Grant	75,000	25,000	0	0	50,000	0	0	0	0
<b>Total Funding</b>	<b>729,000</b>	<b>130,000</b>	<b>0</b>	<b>0</b>	<b>599,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

Project Title

Cascade Town Center Water System Improvements

Project ID

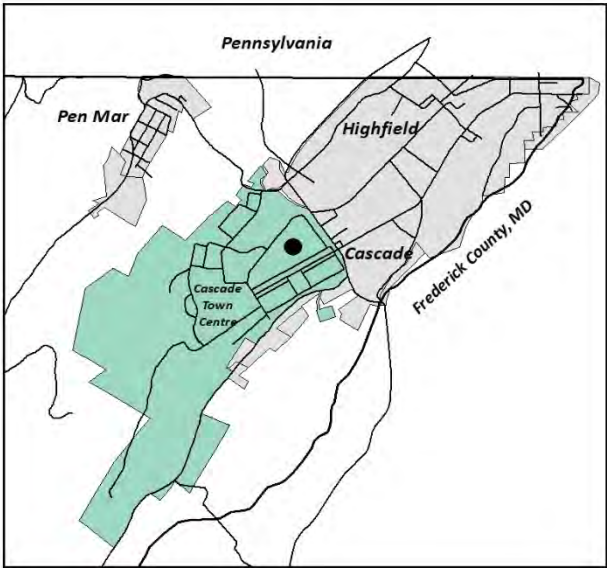
LIN044

Full-time Employees

0

Operating Costs

\$0



Project Description

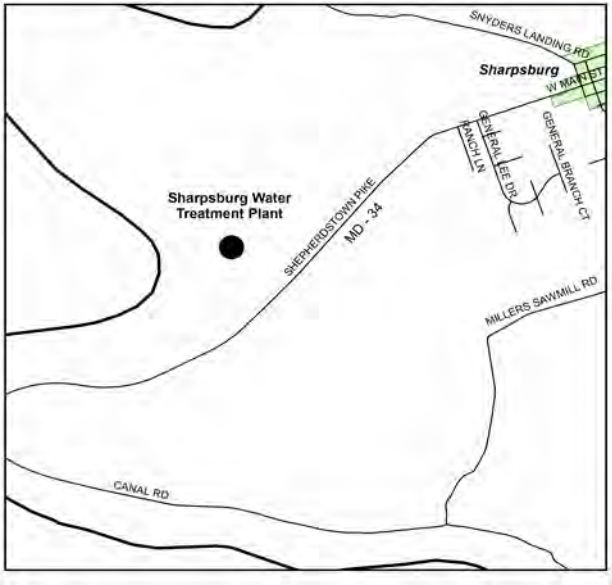
The County now owns and operates the former Ft Ritchie water system. Upgrades need to be made to the system to operate it and to comply with MDE requirements.

Project Justification

Upgrades are needed at the well houses and reservoirs.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Construction	125,000	0	125,000	0	0	0	0	0	0
<b>Total Cost</b>	<b>125,000</b>	<b>0</b>	<b>125,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b><u>Funding Sources:</u></b>									
Self-Supported Bond	75,000	0	75,000	0	0	0	0	0	0
Capital Reserve - General	50,000	0	50,000	0	0	0	0	0	0
<b>Total Funding</b>	<b>125,000</b>	<b>0</b>	<b>125,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

Project Title	Sharpsburg Water Treatment Plant
Project ID	TRP023
Full-time Employees	0
Operating Costs	\$0



**Project Description**      The project includes Sharpsburg Water Treatment Plant upgrades required by Federal and State regulations.

**Project Justification**      Regulations continue to become more restrictive, thus requiring the plant to be upgraded to ensure compliance with Drinking Water Regulations.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	794,000	204,000	0	0	0	0	0	590,000	0
<b>Total Cost</b>	<b>794,000</b>	<b>204,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>590,000</b>	<b>0</b>
<b>Funding Sources:</b>									
Self-Supported Bond	794,000	204,000	0	0	0	0	0	590,000	0
<b>Total Funding</b>	<b>794,000</b>	<b>204,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>590,000</b>	<b>0</b>

Project Title	General WTP Improvements
Project ID	TRP025
Full-time Employees	0
Operating Costs	\$0

**Project Description**      The project involves general upgrades to water treatment plants including electrical systems and equipment.

**Project Justification**      Upgrades are required to address aging equipment, maintain a high level of water quality, and address Federal and State regulations.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b><u>Project Costs:</u></b>									
Construction	1,168,298	66,298	30,000	0	242,000	290,000	0	0	540,000
<b>Total Cost</b>	<b>1,168,298</b>	<b>66,298</b>	<b>30,000</b>	<b>0</b>	<b>242,000</b>	<b>290,000</b>	<b>0</b>	<b>0</b>	<b>540,000</b>
<b><u>Funding Sources:</u></b>									
Cascade Fund	13,000	13,000	0	0	0	0	0	0	0
Water Fund	16,000	16,000	0	0	0	0	0	0	0
Self-Supported Bond	1,139,298	37,298	30,000	0	242,000	290,000	0	0	540,000
<b>Total Funding</b>	<b>1,168,298</b>	<b>66,298</b>	<b>30,000</b>	<b>0</b>	<b>242,000</b>	<b>290,000</b>	<b>0</b>	<b>0</b>	<b>540,000</b>

Project Title

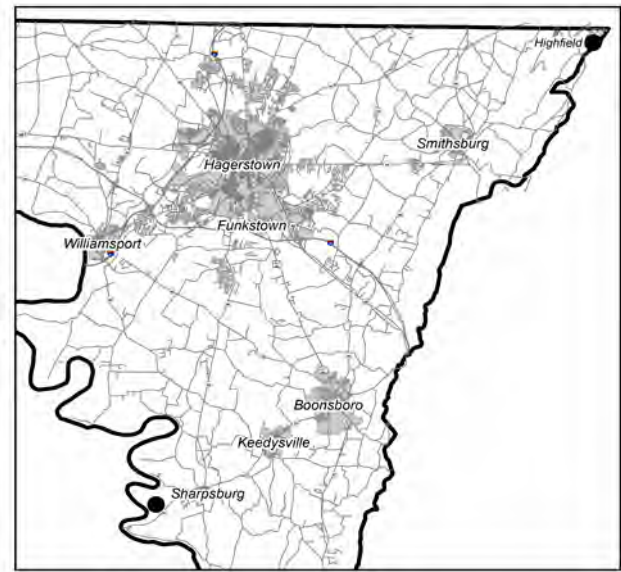
Highfield/Sharpsburg Water Storage Tank

Full-time Employees

0

Operating Costs

\$0



Project Description

The project includes rehabilitation of the existing water storage tanks.

Project Justification

Rehabilitation of the tank structures is required as they age to extend their life expectancy and for the continuation of operations.

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
<b>Project Costs:</b>									
Construction	336,000	0	0	0	0	0	0	0	336,000
<b>Total Cost</b>	<b>336,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>336,000</b>
<b>Funding Sources:</b>									
Self-Supported Bond	336,000	0	0	0	0	0	0	0	336,000
<b>Total Funding</b>	<b>336,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>336,000</b>

Project Title                      WQ Main Replacement

Full-time Employees            0

Operating Costs                \$0

Project Description            The project will replace aging water mains in various water distribution systems.

Project Assumptions           Pending grant funding approval.

Project Justification           Replacement is needed due to the age and deterioration of the water mains.

**Project Costs:**

	Total	Prior Appr.	2021	2022	2023	2024	2025	2026	Future
Construction	1,366,000	0	0	0	566,000	100,000	100,000	0	600,000
<b>Total Cost</b>	<b>1,366,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>566,000</b>	<b>100,000</b>	<b>100,000</b>	<b>0</b>	<b>600,000</b>

**Funding Sources:**

Self-Supported Bond	1,241,000	0	0	0	516,000	75,000	75,000	0	575,000
State Grant	125,000	0	0	0	50,000	25,000	25,000	0	25,000
<b>Total Funding</b>	<b>1,366,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>566,000</b>	<b>100,000</b>	<b>100,000</b>	<b>0</b>	<b>600,000</b>

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